



N64W23760 Main Street

Sussex, Wisconsin 53089

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**AGENDA**  
**VILLAGE OF SUSSEX**  
**PLAN COMMISSION MEETING**  
**6:30 PM TUESDAY, JULY 20, 2021**  
**SUSSEX CIVIC CENTER – BOARD ROOM 2<sup>nd</sup> FLOOR**  
**N64W23760 MAIN STREET**

Pursuant to the requirements of Section 19.84, Wis. Stats., notice is hereby given of a meeting of the Village of Sussex Plan Commission, at which a quorum of the Village Board may attend. Notice of Village Board Quorum, (Chairperson to announce the following if a Village Board quorum is in attendance: Please let the minutes reflect that a quorum of the Village Board is present and that the Village Board members may be making comments during any portion where the public is allowed to comment or if the rules are suspended to allow the same.)

1. Roll call.
2. Consideration and possible action on the Plan Commission minutes of June 15, 2021.
3. Consideration and possible action on Permitted Uses and Plans:
  - A. Consideration and possible action on a Plan of Operation and site plan for Visu-Sewer, Inc. N61W23195 Silver Spring Drive.
  - B. Consideration and possible action on a covered parking structure at The Courtyard South parking lot W235N6350 Hickory Drive.
4. Conditional Use Public Hearing(s) and consideration and possible action on Conditional Use Permits and Plans on the following items:
  - A. Reconvene the public hearing for a Legal Non-Conforming Conditional Use, Plan of Operation and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring.
  - B. Consideration and possible action on a Legal Non-Conforming Conditional Use, Plan of Operation and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring.
5. Consideration and possible action on CSM's, Plats, Zoning and Planning Items:
  - A. Consideration and possible action on Final Plat Addition No. 4 for Sussex Preserve Subdivision.
6. Other items for future discussion.
  - A. Discussion of the B-4 District, Map, Code and Downtown Development Plan.
7. Adjournment.

Anthony LeDonne  
Chairperson

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Jeremy J. Smith  
Village Administrator

Please note that, upon reasonable notice, efforts will be made to accommodate the needs of disabled individuals through appropriate aids/services. For additional information or to request this service, contact the Village Clerk at 246-5200.

DISCLAIMER- THE FOLLOWING ARE DRAFT MINUTES FROM THE  
PLAN COMMISSION AND ARE  
SUBJECT TO CHANGE UPON APPROVAL OF THE PLAN COMMISSION  
**VILLAGE OF SUSSEX**  
**SUSSEX, WISCONSIN**

Minutes of the Plan Commission meeting held on June 15, 2021

President LeDonne called the meeting to order at 6:30 p.m.

Members present: President Anthony LeDonne, Commissioners Jim Muckerheide (arrived at 6:34pm), Roger Johnson, Mike Knapp, Mike Schulist, Deb Anderson and Trustee Greg Zoellick

Others present: Assistant Village Administrator Kelsey McElroy-Anderson, Village Attorney John Macy, Village Engineer Judy Neu, Administrative Assistant Anne Pulvermacher, Deputy Clerk Caren Brustmann and applicants.

A quorum of the Village Board was not present at the meeting.

**Consideration and possible action on the minutes on the Plan Commission meeting of May 18, 2021.**

A motion by Johnson, seconded by Schulist to approve the minutes of the Plan Commission meeting of May 18, 2021 as presented. Motion carried 7-0

**Consideration and possible action on a Plan of Operation and site plan for Milwaukee Sign Company N63W22625 Main Street.**

Applicant Kelly Hanaway, N63W22625 Main St, Sussex, provided a brief plan of operation.

Mrs. McElroy-Anderson reviewed the Plan Staff Memo (copy attached); stating this site is zoned M-1. The general manufacturing and assembling of signs is a permitted use in accordance with Section 17.0422 (A)(4)(f) in the M-1 Industrial District. Milwaukee Sign Company will occupy 3,200 square feet in the 36,200 square foot building. Hours of operation will be Monday through Friday 8:00 a.m. to 5:00 p.m. and Saturday 8:00 a.m. to noon. This site is not allowed any outside storage. This site has 72 parking spaces with 38 allocated to other users. This user requires 4 per code. Any signage for this site will need to be approved by the building inspector.

Plan Commission Comments: None

Public Comments: None

A motion by Anderson, seconded by Muckerheide to approve the Plan of Operation and site plan for Milwaukee Sign Company N63W22625 Main Street; a finding the use and structures meet the principals of 17.1002(A-H), subject to the standard conditions of Exhibit A.

Motion carried 7-0

**Consideration and possible action on a Plan of Operation and site plan for Tap In N65W24838 Main Street.**

Applicants Lori & Brett Flora, N65W24838 Main St, Sussex, provided a brief plan of operation.

Mrs. McElroy-Anderson reviewed the Plan Staff Memo (copy attached); stating this site is zoned B-2. The entertainment facility with a bar is a permitted use in accordance with Section 17.0417 (A)(1)(c) and (2)(b). At the May Plan Commission a code change was approved to allow entertainment facilities in spaces under 30,000 square N64W23760 Main Street Sussex, Wisconsin 53089 Phone (262) 246-5200 FAX (262) 246-5222 Email: info@villagesussex.org Website: www.villagesussex.org Plan Staff Memo for Plan Commission June 15, 2021 2



feet which will now allow this user. Tap In will have golf simulators and serve alcohol and simple food options. The owner still will need to obtain approval for a liquor license with the Village. Hours of operation will be Monday through Thursday 12:00 p.m. to 10:00 p.m., Friday 12:00 p.m. to 11:00 p.m., Saturday 10:00 a.m. to 11:00 p.m. and Sunday 10:00 a.m. to 10:00 p.m. This site meets the parking requirements according to Section 17.0603 (2)(r). If there is any outdoor patio an outdoor establishment permit needs to be applied for. Any signage for this site will need to be approved by the building inspector.

Plan Commission Comments: Trustee Zoellick inquired if this business was family orientated. Mrs. Flora expressed Tap In would be family orientated. Commissioner Johnson inquired how many golf courses would be provided with the software. Mr. Flora expressed there will be over 88 different courses offered with their purchased software.

Public Comments: None

A motion by Johnson, seconded by Zoellick to approve the Plan of Operation site plan for Tap In N65W24838 Main Street; a finding that the use and structures meet the principals of 17.1002(A-H), subject to the standard conditions of Exhibit A. Motion carried 7-0

**Convene the public hearing for a Legal Non-Conforming Conditional Use and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring.**

Applicants Bishnu Adhikari & Ram Subedi, N62W23456 Silver Spring Dr., Sussex, provided a brief plan of operation.

Mrs. McElroy-Anderson reviewed the Plan Staff Memo (copy attached); stating this site is zoned B-4. The gas station with car wash is a legal non-conforming use in accordance with Section 17.0507. The business and operations are being purchased by a new owner. Mostly the operation will remain the same; however, the buyer is requesting to add the sale of beer and liquor and use the location for Uhaul rental and moving boxes. Hours of operation will be Monday – Friday, 5:00 a.m. to 9:00 p.m., Saturday 6:00 a.m. to 9:00 p.m. and Sunday 7:00 a.m. to 9 p.m. Gas pumps are available 24 hours 7 days a week. Please see the attached impact report for more information.

The Petitioner will need to prove the standards/conditions in the attached memo during the Public Hearing process. The Plan Commission may add additional conditions for the Petitioner to meet in the Conditional Use. The shaded areas (if any) in the attached memo reflect items in the code that are unlikely to be meritorious to the application or are administrative in nature.

Plan Commission Comments: Commissioner Knapp stated that the potholes in the property's parking lot are unacceptable and requested it be repaved. Mr. Subedi noted a contract is in place for the repaving. Trustee Zoellick expressed noise concern generated from the Uhails being returned at all hours of the night. Mr. Subedi noted per their contract with Uhaul, only four vehicles/trailers total are allowed on their property per 24 hours and there is a lock box for the drop offs.

After further discussion, the Commissioners requested the Petitioner provide a more detailed site plan to include the Uhaul designated area with requested maximum number of Uhails allowed on site (including overflow), lighting information/security camera placement, as well as the contract for the repaving of the parking lot.

Public Comments: None

Mrs. McElroy-Anderson stated that staff has found the application complete and recommends scheduling the Public Hearing.

A motion by Johnson, seconded by Anderson to direct staff to prepare a Conditional Use Order with specific

requested considerations and to adjourn the public hearing for 6:30 p.m. on July 20, 2021 to further review the CU order. Motion carried 7-0

**Consideration and possible action on Final Plat Addition No. 1 for Woodland Trails Subdivision.**

Mrs. McElroy-Anderson reviewed the Plan Staff Memo (copy attached); stating this site is zoned RS-3 with a PDO. This Final Plat is part of the next phase for the 91 single family subdivision. The approval of this Plat is for next single family lots with road and utility improvements. Building permits may be issued once staff is satisfied all the conditions of the Developers Agreement and Chapter 18 are met and once the Plat is recorded with the Registry of Deeds. The subdivision still has 28 single family lots remaining to be platted.

Plan Commission Comments: None

Public Comments: None

A motion by Schulist, seconded by Knapp to recommend to the Village Board approval of Final Plat Addition No. 1 for Woodland Trails Subdivision conditioned upon the obligations of the Developers Agreement being met, final review by the Village Engineer and subject to the standard conditions of Plat approvals and Exhibit A.

Motion carried 7-0

**Consideration and possible action on a CSM for purchase transfer of well house #3 W233N6048 Lilac Drive and vacate the pedestrian walkway to abutting property owners.**

Mrs. McElroy-Anderson stated this CSM will be the final step in the transfer of the well house #3 and the pedestrian walkway to the abutting property owners. Payment for the well house has been received.

Plan Commission Comments: None

Public Comments: None

A motion by Anderson, seconded by Schulist, to recommend approval of the CSM to the Village Board for transfer of well house #3 W233N6048 Lilac Drive and to vacate the pedestrian walkway subject to review by the Village Engineer, the standard conditions of CSM approval and Exhibit A.

Motion carried 7-0

**Other items for future discussion**

Commissioner Johnson would like the CDA to review the zoning requirements along Main Street as well as vacant areas for future development and recommend considerations to the Plan Commission.

**Adjournment:**

A motion by LeDonne, seconded by Schulist to adjourn the meeting at 7:17 p.m.

Motion carried 7-0

Respectfully Submitted,

Caren Brustmann  
Deputy Clerk

## **Exhibit “A”**

### **Village of Sussex Plan Commission**

#### **Standard Conditions of Approval Plan of Operation and Site Plan**

The Plan Commission for the Village of Sussex authorizes the Building Inspector to issue a building permit to the Petitioner and approves the general layout, architectural plans, ingress and egress, parking, loading and unloading, landscaping, open space utilization, site plan and plan of operation subject to the following conditions:

1. Presentation compliance. Subject to Petitioner operating the premises at all times in substantial conformity with the presentation made to the Village Plan Commission, as modified or further restricted by the comments or concerns of the Village Plan Commission.
2. Inspection compliance. Subject to the Petitioner submitting to and receiving the approval from the Village Administrator, written proof that the Village Building Inspector and Fire Chief have inspected the subject property and have found that the subject property is in substantial compliance with applicable federal, State, and local laws, statutes, codes, ordinances, policies, guidelines and best management practices, prior to this approval being effective.
3. Regulatory compliance. Subject to the Petitioner and Owner fully complying with all Village, County of Waukesha, State of Wisconsin and federal government codes, ordinances, statutes, rules, regulations and orders regarding the premises, including but not limited to compliance with Section 17.1000 of the Village of Sussex Zoning Code entitled “Site Plan Review and Architectural Control,” as determined by Village Staff.
4. Satisfaction of Engineer. Subject to the Developer satisfying all comments, conditions, and concerns of the Village Engineer regarding the Petitioner’s application prior to this approval being effective.
5. Required plans. Subject to the Developer submitting to and receiving written approval from the Village Administrator of all of the following plans as deemed necessary by the Village Administrator:
  - A. Landscaping plan
  - B. Parking plan
  - C. Lighting plan
  - D. Signage plan
  - E. Traffic plan
  - F. Grading plan

**Amendment approved at the Plan Commission meeting on July 17, 2014**

- G. Tree preservation plan
- H. Open space plan
- I. Water plan
- J. Surface and stormwater management plan
- K. Sewer plan
- L. Erosion control plan
- M. \_\_\_\_\_

N. \_\_\_\_\_  
O. \_\_\_\_\_  
\_\_\_\_\_

P.

6. Screening of All Dumpsters. Subject to the Petitioner and Owner screening all dumpsters as required by the ordinance to the satisfaction of the Village Administrator.

7. Payment and reimbursement of fees and expenses. Subject to the Petitioner and Owner paying all costs, assessments and charges due and owing to the Village of Sussex either by the Petitioner or imposed on the subject property, including, but not limited to, real estate taxes, personal property taxes, utility bills, special assessments, permit fees, license fees and professional fees which shall include all costs and expenses of any type that the Village incurs in connection with Petitioner's application, including the cost of professional services incurred by the Village (including engineering, legal and other consulting fees) for the review of and preparation of the conditions of approval, attendance at meetings or other related professional services for this application, as well as for any actions the Village is required to take to enforce any of the conditions of this approval due to a violation of these conditions by the Petitioner or the Owner, as authorized by law.

8. Condition if the Property is in the B-4 Central Business District. If the property is in the B-4 Central Business District, the Petitioner shall comply with the standards and conditions found within the Village of Sussex Downtown Design and Development Plan and other plans as may be approved from time to time by the Community Development Authority in its role as a Redevelopment Authority to guide development within the Village's Downtown.

9. Subject to acceptance. The Owner by requesting a permit either directly or through an agent, and accepting the same is acknowledging that they have received a copy of this conditional approval, that they understand and accept the same, and that upon failure to satisfy these conditions this approval is void, and the same is deemed to not have been approved, and the Petitioner will therefore need to re-commence the application process.

10. Any official named in this document can appoint a designee to perform his or her duties.





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## MEMORANDUM

TO: Plan Commission  
FROM: Jeremy Smith, Village Administrator  
RE: Plan Commission meeting of July 20, 2021  
DATE: July 13, 2021

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All Code Sections in this memo refer to the Sussex Municipal Code Chapter 17 version dated March 25, 2014 with subsequent amendments thereto.

01. **Roll call.**
02. **Consideration and possible action on the minutes of the Plan Commission meeting of June 15, 2021.**
03. **Consideration and possible action on Permitted Uses and Site Plans:**
  - A. **Consideration and possible action on a Plan of Operation and site plan for Visu-Sewer, Inc. N61W23195 Silver Spring Drive.**

This site is zoned B-3. The general Construction services contractor is a permitted use in accordance with Section 17.0418 (A)(5)(j) in the B-3 Highway Business District. Visu-Sewer will purchase the 12,500 square foot building previously owned by Zell Machine. Hours of operation will be Monday through Friday 7:00 a.m. to 5:00 p.m. This site is not allowed any outside storage. This site has 6 parking spaces with and per code they should have 6 stalls.

Visu-Sewer plans to put a new roof on the building, reside the building, replace the garage doors, new outdoor lighting, fix up the landscaping, and clean the property up as needed

Any signage for this site will need to be approved by the building inspector.

### **Policy Question:**

1. Are there any concerns with the Plan of Operation?
2. Are there any concerns with the property?

### **Action Items:**

1. Act on the plan of operation.

**Staff Recommendation:** Staff recommends the Plan Commission approve the Plan of Operation and site plan for Visu-Sewer, Inc. N61W23195 Silver Spring Drive; a finding the use and structures meet the principals of 17.1002(A-H), subject to the standard conditions of Exhibit A.

**B. Consideration and possible action on a covered parking structure at The Courtyard south parking lot W235N6350 Hickory Drive.**

This site is zoned B-4. The Courtyard plans and design have previously been approved. A request from the owner has been presented for a covered accessory structure over eleven stalls on the south side of the building for the tenants to park their cars under. This type of structure can be allowed in the B-4, it is up to the Plan Commission to determine if the structure as designed meets the standards of consistent with the building architecture and meets the intent of the design standards.

**Policy Question:**

1. Are there any concerns with the accessory structure?

**Action Items:**

1. Act on the plan for the accessory structure.

**Staff Recommendation:** Staff does not have a recommendation on this matter.

The Plan Commission could approve the plan for a covered accessory structure at the south parking lot for The Courtyard at W235N6350 Hickory Drive; a finding that the use and structures meet the principals of 17.1002(A-H), subject to the standard conditions of Exhibit A, or

The Plan Commission could send the plan to the ARB for their review and comment, or

The Plan Commission could deny the request to add the accessory structure as presented for failure to be consistent with the architectural design of the main building and for not meeting the design standards of the Village.

**04. Conditional Use Public Hearing(s) and consideration and possible action on Conditional Use Permits and Plans on the following items:**

**A. Convene the public hearing for a Legal Non-Conforming Conditional Use Plan of Operation and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring.**

This site is zoned B-4. The gas station with car wash is a legal non-conforming use in accordance with Section 17.0507. The business and operations are being purchased by a new owner. Mostly the operation will remain the same; however, the buyer is requesting to add the sale of beer and liquor and use the location for Uhaul rental and moving boxes. Hours of operation will be Monday – Friday, 5:00 a.m. to 9:00 p.m., Saturday 6:00 a.m. to 9:00 p.m. and Sunday 7:00 a.m. to 9 p.m. Gas pumps are available 24 hours 7 days a week.

**The Conditional Use Permit Standards to be met:**

A. This legal non-conforming conditional use is granted for Sussex Convenience Inc. for the above enumerated uses, subject to the following conditions:

1. Presentation Compliance. The vehicle washing and 24-hour gas station is approved herein on the Subject Property, and the use thereof, shall be used in substantial conformity

with the presentation at the Public Hearings before the Plan Commission held on June 15, 2021 and July 20, 2021.

2. Subject Property. This legal non-conforming conditional use permit issued to the Petitioner, shall be limited to the property described in Exhibit "A" attached hereto and incorporated herein as presented at the public hearing.

3. Plans. The Petitioner is required and must have all plans current, approved by the Plan Commission for the Village of Sussex, and on file with the Plan Commission for the Village of Sussex in order for this LNCU to be in effect. The Petitioner shall be entitled to amend or change any plan contemplated herein subject to the aforementioned conditions and subject to the Plan Commission for the Village of Sussex approval and without a public hearing, if such amendments and/or change is not a substantial amendment or change in any plan contemplated herein that the Plan Commission for the Village of Sussex feels, in its sole discretion, to be substantial will require a new permit and all Village procedures in place at the time must be followed.

A. Site Plan. The Petitioner shall maintain the existing site plan, which shall be attached hereto and incorporated herein as Exhibit B.

B. Plan of Operation. The Petitioner shall submit to and receive approval from the Village Plan Commission a specific plan of operation for the Subject Property, the same to be in substantial conformance with the preliminary plans on file and with the presentation at the meeting. Once approved the plan of operation shall be attached hereto and incorporated herein as Exhibit C.

1. The hours of operation for the gas pumps will be 24 hours. While it is anticipated there will be no issues with the 24-hour operation this provision is specifically highlighted for reconsideration by the Plan Commission should concerns or problems arise related to the extended hours of operation.

2. The Gas station may apply for a license to sell beer, wine, and or liquor and may sell said products, if approval by the Village is granted only within strict compliance of said license and the conditions of approval of the same, and with compliance to any amendments to said license(s) made over time.

C. Traffic, Access and Loading Plans. The petitioner shall comply with the specific traffic, access, loading, and egress plan on file with the Village Clerk.

D. Lighting Plan. The petitioner shall adhere to the lighting plan on file for the subject property. No light rays shall be directed onto neighboring properties.

E. Signage Plan. All signage on the subject property shall conform to the requirements set forth in Section 17.0800 of the Village Zoning Ordinance and the Design Standards of the Village of Sussex, except the two existing pole signs which may exist with what rights they may have as legal nonconforming signs. No change in signage shall be permitted until approved by the Architectural Review Board and Building Inspector for the Village of Sussex.



1. The site has two existing pole signs that may have legal nonconforming rights. The sign face may be changed on these pole signs without a public hearing. If either or both of these pole sign structures need to be replaced or modified the signage plan for the entire site must come into compliance with the then current sign code as a condition of granting this legal non-conforming conditional use. Compliance with this standard if modification or replacement is sought may and is likely to require the removal of the pole signs in their entirety.

F. Sewer, Water, Stormwater and Erosion Control Plans. The Petitioner shall comply with the sewer, water, stormwater and erosion control plans on file with the Village Clerk.

G. Outdoor Storage Plan. No outdoor storage shall be permitted on the subject property with the following specific exceptions:

1. Trash dumpsters limited to the minimum number needed to meet the recycling law requirements. The trash dumpsters shall be screened from view and shall be maintained in a safe and sanitary condition at all times.
2. The storage and sale of propane tank in the style, manner and location of the outdoor storage plan on file with the Village Clerk.
3. No other outside storage or sales of merchandise from the above listed materials (excluding gasoline for vehicle usage) are permitted outside of the building.
4. A limit of ten parking stalls for the storage/rental of Uhaul trucks and trailers.

4. Adult-Oriented Materials. No adult-oriented materials or pornographic videotapes, magazines, or gift items will be sold or rented from the subject facility.

5. Licenses. The Petitioner shall be required to obtain any and all required licenses and permits from the Village, County, State, and Federal Government. If any license or permit is issued, any and all conditions of the same are incorporated herein and made a part of this legal non-conforming conditional use permit.

6. Laws. The Petitioner shall comply with all Federal, State, County, and local rules, codes, ordinances, and regulations in the construction, operation, and maintenance of the Subject Property.

7. Building and Fire Inspection. The Petitioner is required to keep the Subject Property in compliance with all federal, State, and local laws, statutes, codes, ordinances, policies, and guidelines as determined by the Building and Fire Inspectors of the Village of Sussex.

8. Aesthetics. The Petitioner is required to properly maintain the Subject Property at all times and in full compliance with the Village Property Maintenance Code, Chapter 9 of the Village of Sussex Code all to the satisfaction of the Plan Commission for the Village of Sussex.

9. Junk. No junk shall be accumulated or stored on the Subject Property. No burying or burning of junk is permitted on the Subject Property.

10. Temporary Use or Activity. No temporary use or special activity or event shall be permitted without prior approval of the Plan Commission of the Village of Sussex and the same must be in compliance with all ordinance, rules, and regulations of the Village of Sussex and all necessary permits must be obtained.

11. Fees and Expenses. The Petitioner, upon issuance of this legal non-conforming conditional use permit, shall reimburse the Village of Sussex for all expenses incurred by the Village, including, but not limited to, expenses for the Village Administrator, Village Engineer, Village Attorney, and all other professionals and technical assistance realized by the Village in approving and granting this legal non-conforming conditional use permit. The Village Clerk shall provide the Petitioner with copies of all itemized invoices.

12. Enforcement. Any attorney fees incurred by the Village of Sussex to enforce any of the conditions or requirements of this legal non-conforming conditional use permit must be paid by the Petitioner.

13. Complaints. In the event the Petitioner receives any complaints with regard to the operation authorized by this legal non-conforming conditional use, the Petitioner shall respond to such complaints in writing within a reasonable time not to exceed two weeks from the date of the complaint and shall provide a copy of the written response to the Sussex Village Administrator within the same period of time. If the complaint was made in writing, the copy provided to the Village Administrator shall include a copy of the complaint.

14. No Nuisance. The Village reserves the right to rescind its approval of this legal non-conforming conditional use permit based upon the finding that the use is incompatible and a nuisance to surrounding uses, that the use is not in the public interest, or that the use adversely affects the use of adjacent lands, provided the Petitioner is given an opportunity to be heard on the matter and, if so rescinded the Petitioner and Subject Property shall thereupon be immediately subject to the Village of Sussex Zoning Ordinances, as applicable, regarding the use of the Subject Property as though no legal non-conforming conditional use permit was granted.

15. Subject to Acceptance. Subject to the Owner approving in writing the issuance of the same and Petitioner acknowledging in writing that they have received a copy of this legal non-conforming conditional approval, that they understand and accept the same, and that upon failure to satisfy these conditions this approval is void, and the same is deemed to not have been approved, and the Petitioner will therefore need to re-commence the application process.

16. Review. The Plan Commission for the Village of Sussex reserves its right to review the operation and amend the legal non-conforming conditional use permit as the Plan Commission for the Village of Sussex deems appropriate

B. Any use not specifically listed as permitted shall be considered to be prohibited except as may be otherwise specifically provided herein. In case of a question as to the classification of use, the question shall be submitted to the Plan Commission for the Village of Sussex for determination.

C. No use is hereby authorized unless the use is conducted in a lawful, orderly and peaceful manner. Nothing in this order shall be deemed to authorize any public or private nuisance or to constitute a waiver, exemption or exception to any law, ordinance, order or rule of either the Village of Sussex, the County of Waukesha, the State of Wisconsin, the United States of America or other duly constituted authority, except only to the extent that it authorizes the use of the subject property above described in any specific respects

described herein. This order shall not be deemed to constitute a building permit, nor shall this order constitute any other license or permit required by Village ordinance or other law.

D. This legal non-conforming conditional use hereby authorized shall be confined to the subject property described, without extension or expansion other than as noted herein, and shall not vary from the purposes herein mentioned unless expressly authorized in writing by the Plan Commission for the Village of Sussex as being in compliance with all pertinent ordinances.

E. Should the permitted legal non-conforming conditional use be abandoned in any manner, or discontinued in use for twelve (12) months, or continued other than in strict conformity with the conditions of the original approval, or should the petitioner be delinquent in payment of any monies due and owing to the municipality, or should a change in the character of the surrounding area or the use itself cause it to be no longer compatible with the surrounding area or for similar cause based upon consideration of public health, safety or welfare, the legal non-conforming conditional use may be terminated by action of the Plan Commission for the Village of Sussex.

F. Any change, addition, modification, alteration and/or amendment of any aspect of this legal non-conforming conditional use, including but not limited to a change in the use, premises, lands or owners, other than as specifically authorized herein, shall require a new permit and all Village procedures in place at the time must be followed.

G. Unless this legal non-conforming conditional use permit expressly states otherwise, plans that are specifically required by this legal non-conforming conditional use order may be amended upon the prior approval of the Plan Commission if the Plan Commission finds the plan amendment to be minor and consistent with the legal non-conforming conditional use permit. Any change in any plan that the Plan Commission, in its sole discretion, finds to be substantial shall require a new permit, and all procedures in place at the time must be followed.

H. Should any paragraph or phrase of this legal non-conforming conditional use permit be determined by a Court to be unlawful, illegal or unconstitutional, said determination as to the particular phrase or paragraph shall not void the rest of the legal non-conforming conditional use and the remainder shall continue in full force and effect.

I. This legal non-conforming conditional use permit shall be effective for an initial term that ends five years from the date of January 1 of the year the legal non-conforming conditional use is approved. During the fifth year of the LNCU the Village Administrator or designee shall examine the record to determine if concerns about the operation have been raised in writing by the public, a governmental body or official, or anyone else, and shall also review the site and the adjoining area for compliance with the conditions of the LNCU. If based upon that review the use of the property is compatible with the surrounding areas and the petitioner is in substantial compliance with all terms of this legal non-conforming conditional use agreement, then, in that event, the legal non-conforming conditional use can be automatically renewed for another 5-year term. If based upon that review the Village Administrator finds concerns about compliance with the conditions of the legal non-conforming conditional use, the petitioner shall be brought back before the

Plan Commission for consideration of the renewal. The Petitioner shall have the responsibility to apply for the renewal by January 30th of the fifth (5) year of the term. Failure of Petitioner to apply for the renewal as provided herein shall be deemed a violation of the conditions of the legal non-conforming conditional use and may serve as a basis for termination of the legal non-conforming conditional use permit. The Plan Commission for the Village of Sussex may add additional conditions at any time.

1. Where the changing character of the surrounding area causes the original legal non-conforming conditional use or subsequent approved amendments thereto to no longer be compatible with the surrounding area, or for similar cause, based upon consideration for the public welfare, the legal non-conforming conditional use order and any subsequent approved amendments or changes may be terminated by action of the Plan Commission of the Village of Sussex. Such use shall thereafter be classified as a legal nonconforming use as it was permitted to exist on the day it was terminated.

2. Where this permitted legal non-conforming conditional use does not continue in conformity with the conditions of the original approval or subsequent approved amendments or changes, the legal non-conforming conditional use grant and any subsequent approved amendments thereto may be amended or terminated by action of the Plan Commission for the Village of Sussex. The Plan Commission for the Village of Sussex may require complete termination of such use.

3. This legal non-conforming conditional use may be reviewed annually. Additionally, this conditional use may be reviewed by the Plan Commission for the Village of Sussex at any time upon complaint or upon Plan Commission initiative.

J. Upon acceptance by petitioner of this legal non-conforming conditional use permit, all prior legal non-conforming conditional use permits granted to the subject property are hereby revoked and terminated.

K. If any aspect of this legal non-conforming conditional use permit or any aspect of any plan contemplated and approved under this legal non-conforming conditional use is in conflict with any other aspect of the legal non-conforming conditional use or any aspect of any plan of the legal non-conforming conditional use, the more restrictive provision shall be controlling as determined by the Plan Commission for the Village of Sussex.

L. If any paragraph or phrase of this legal non-conforming conditional use order is declared by a Court of competent jurisdiction to be invalid, unlawful or unenforceable, such decision shall apply only to the specific paragraph or phrase thereof directly specified in the decision and shall not affect the validity of any other provisions, sections or portions thereof of this order. The remainder of the order shall remain in full force and effect.  
Comments from the public if any.

#### **Additional Plan Commission conditions:**

The Petitioner, Public and Plan Commission will have a chance to present evidence and discuss the Conditional Use Order.

#### **Policy Questions:**

1. Is there any further unanswered questions by the Plan Commission or any other items requiring a continuation of the Public Hearing?

**Action:**

1. Close the public hearing or continue the public hearing with specific information sought by the Petitioner/Public.

**B. Consideration and possible action on a Legal Non-Conforming Conditional Use, Plan of Operation and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring Drive.**

Staff has prepared a conditional use document for consideration at this meeting. The Plan Commission has heard testimony on the application for the Legal Non-Conforming Conditional Use and site plan for Sussex Convenience, gas station and car wash.

**Policy Questions:** Has the Petitioner provided substantial evidence proving they meet the standards/regulations of the Ordinance, the conditions and standards of the Conditional Use Permit, and the other conditions as set forth by the Plan Commission? If No, the deficiency will need to be detailed.

Has the Petitioner provided substantial evidence with regards to the following Standards/Regulations?

**The Application is complete and consistent with 17.0502** Yes or No

**The use(s) and plans are compliant with 17.0503 (Review of CU's)** Yes or No

**The use(s) and plans are compliant with 17.0200 (General Conditions)** Yes or No

**The uses(s) and plans are compliant with 17.0419 B-4 Central Mixed Use** Yes or No

**The uses(s) and plans are compliant with 17. 1000 (Site Plan Review)** Yes or No

**Has the Petitioner provided substantial evidence and adequately addressed the findings of the impact report per 17.0506.A.** Yes or No

Has the Petitioner provided substantial evidence with regards to the Conditional Use Permit as follows:

**B.3.A. Site Plan Standards compliance** Yes or No

**A.3.B. Plan of Operation compliance** Yes or No

**A.3.C.-G. Various Plan(s) compliance** Yes or No

**A.4.-16. CU condition compliance** Yes or No

**B-L. Administrative CU Condition compliance** Yes or No

**Action Items:**

1. Act on the CU and site plan.

**Staff Recommendation:** Staff recommends approval of the Legal Non-Conforming Conditional Use based upon the evidence presented at the Public Hearings, Plan of Operation and site plan for Sussex Convenience, gas station and car wash N62W23456 Silver Spring; a finding the use and structures meet the principals of 17.1002(A-H); subject to the standard conditions of Exhibit A.

**05. Consideration and possible action on CSM's, Plats, Zoning, and Planning Items:**

**A. Consideration and possible action on Final Plat Addition No. 4 for Sussex Preserve Subdivision.**

This site is zoned RS-3 with a PDO. This Final Plat Addition No.4 is part of the next phase for platting 14 single family lots in the Sussex Preserve subdivision. The approval of this Plat is for lots with road and utility improvements. Building permits may be issued once staff is satisfied all the conditions of the Developers Agreement and Chapter 18 are met and once the Plat is recorded with the Registry of Deeds.

The subdivision still has 44 single family lots remaining to be platted.

**Policy Question:**

1. Are there any concerns with the Plat?

**Action Items:**

1. Act on the Plat.

**Staff Recommendation:** Staff recommends the Plan Commission recommend to the Village Board approval of Final Plat Addition No. 4 for Sussex Preserve Subdivision conditioned upon the obligations of the Developers Agreement being met, final review by the Village Engineer and subject to the standard conditions of Plat approvals and Exhibit A.

**B. Discuss the B-4 District, Map, Code, and Downtown Design Plan**

Staff will walk through the existing Code, uses, and plan for the Downtown.

No action is required with this topic.

**06. Other Items for future discussion.**

**07. Adjournment.**



## PLAN OF OPERATION

To be used for a business with changes or new business in an existing building.

Is this request to be considered for a Conditional Use? Yes If yes, is this a new CU? Yes

(Conditional Use Permits require a Public Hearing)

OR an amendment to a CU? \_\_\_\_\_

Tax Key # SUXV 0245 989 002

Zoning: B-3

Address of Tenant Space: N61 W23195 Silver Spring Drive

### 1. Name of Business: Visu-Sewer, Inc.

Business		
W230 N4855 Belker Drive	Pewaukee, WI 53072	262-695-2340
Address	City, State, Zip	Phone #
262-695-2359	kelth@visu-sewer.com	
Fax #	Email address	

### 2. Business owner contact information:

Keith M. Alexander

Contact		
W230 N4855 Belker Drive	Pewaukee, WI 53072	262-695-2340
Address	City, State, Zip	Phone #
262-695-2359	kelth@visu-sewer.com	
Fax #	Email address	

### 3. Building/Land owner contact information:

Alex Rentals LLP - Keith Alexander, Managing Member

Contact		
W230 N4855 Belker Drive	Pewaukee, WI 53072	262-695-2340
Address	City, State, Zip	Phone #
262-695-2359	kelth@visu-sewer.com	
Fax #	Email address	

4. Number of Employees/Shifts: 1-2 one - first shift  
Employees Shifts

### 5. Days of Operation:

Put an X in box that applies:

#### Hours

Open for business

Preparation/cleaning

Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
X	X	X	X	X		
7:00am - 5:00pm	7:00am - 5:00pm	7:00am - 5:00pm	7:00am - 5:00pm	7:00am - 5:00pm		

6. Is this an extension of an existing operation? yes but another site
7. On a separate sheet of paper explain your business use and/or the change to your business. Attach a list of all items to be sold, produced or stored on the premises.
8. Is a liquor license or any other special license to be obtained from the Village Board or State Licensing Agencies? No Do you need an Outdoor Establishment Permit? No  
If yes, explain: \_\_\_\_\_  
If yes, please obtain and complete permit application.
9. If your proposed operation will utilize a liquor license, what types of entertainment are you proposing? N/A
10. Do you feel there will be any problems such as odor, smoke or noise resulting from this operation? No If yes, explain: \_\_\_\_\_
11. Dimension of area to be occupied \_\_\_\_\_ Total square footage 12,500 sq. ft.  
If applicable list square footage according to 1<sup>st</sup> floor 12,500 2<sup>nd</sup> floor \_\_\_\_\_

Please provide a copy of the site plan showing parking and layout of the building and if a multi tenant building, label the space which will be occupied by your business.

12. Parking:  
Total Number of Parking Spaces 6 Number of spaces needed per code \_\_\_\_\_  
Number of spaces allocated for employee parking \_\_\_\_\_  
Dimensions of parking lot 40x40 Is parking lot paved? yes

13. Signage: What type of signage are you proposing for your business?

N/A

If applicable, complete a Sign Permit application and submit to the Building Inspection Dept. Please refer to Chapter 17.0800.

I have reviewed a copy of the Zoning Ordinance in the Village of Sussex and hereby agree to abide by the same, as well as any and all terms and conditions of any permit issued pursuant to this application.

I do swear to or affirm that all statements contained herein are true and correct to the best of our knowledge.

Kath M. Alexander  
Name

4/28/21  
Date

Premier / Managing member  
Title or Position

I am aware and approve of the business to be operating in the building owned by \_\_\_\_\_.

\_\_\_\_\_  
Name

\_\_\_\_\_  
Date

\_\_\_\_\_  
Title or Position

**Print Form**

**Clear Form**





June 28, 2021

[www.visu-sewer.com](http://www.visu-sewer.com)

Ms. Kasey Fluet  
Village of Sussex  
N64 W23760 Main Street  
Sussex, WI 53089

Re: Property at N61 W23195 Silver Spring Drive  
Tax Key# SUXV 0245 989 002

Ms. Fluet:

In follow up to our communication over the phone and this morning you will find a brief explanation of Visu-Sewer and our plan of operation at the above address.

Visu-Sewer has been in business for 46 year and we are in the pipe line maintenance and rehabilitation industry. We have seven divisional offices with roughly 240 employees in the Midwest and Mid-Atlantic region. Our investment into people in number one and number two is fleet and equipment management with the need to keep our equipment in a safe clean environment when not on projects sites.

Our plan for the above address is indoor storage for trucks, bypass pumps, generators, and air compressors. All of which are rubber tired and can be driven into or trailered into the facility. We have a full-service shop at our Pewaukee location and would bring equipment over for any maintenance or repair work. The site on Silver Spring would be strictly storage, no sales or production out of this facility.

Corporate Office: W230 N4855 Betker Road (Town of Lisbon)  
Owner of Building: Alex Rentals LLP. (Partnership that owns all our buildings)  
Tenant: Visu-Sewer, Inc. (Owner operator relationship)

Should you have any additional questions I can be reached at 262-695-2340. Thank you in advance for your consideration on our plan of operation.

Respectfully –

VISU-SEWER, INC.



KEITH M. ALEXANDER

**Visu-Sewer, Inc.**

W230 N4855 Betker Dr, Pewaukee, WI 53072 (P) 800-876-8478 / 262-695-2340 (F) 262-695-2359

**Equal Employment Opportunity/Affirmative Action Employer**







## Village of Sussex

### DISCLAIMER:

This map is not a survey of the actual boundary of any property this map depicts.

The Village of Sussex does not guarantee the accuracy of the material contained here in and is not responsible for any misuse or misrepresentation of this information or its derivatives.



SCALE: 1 = 83'



## Village of Sussex

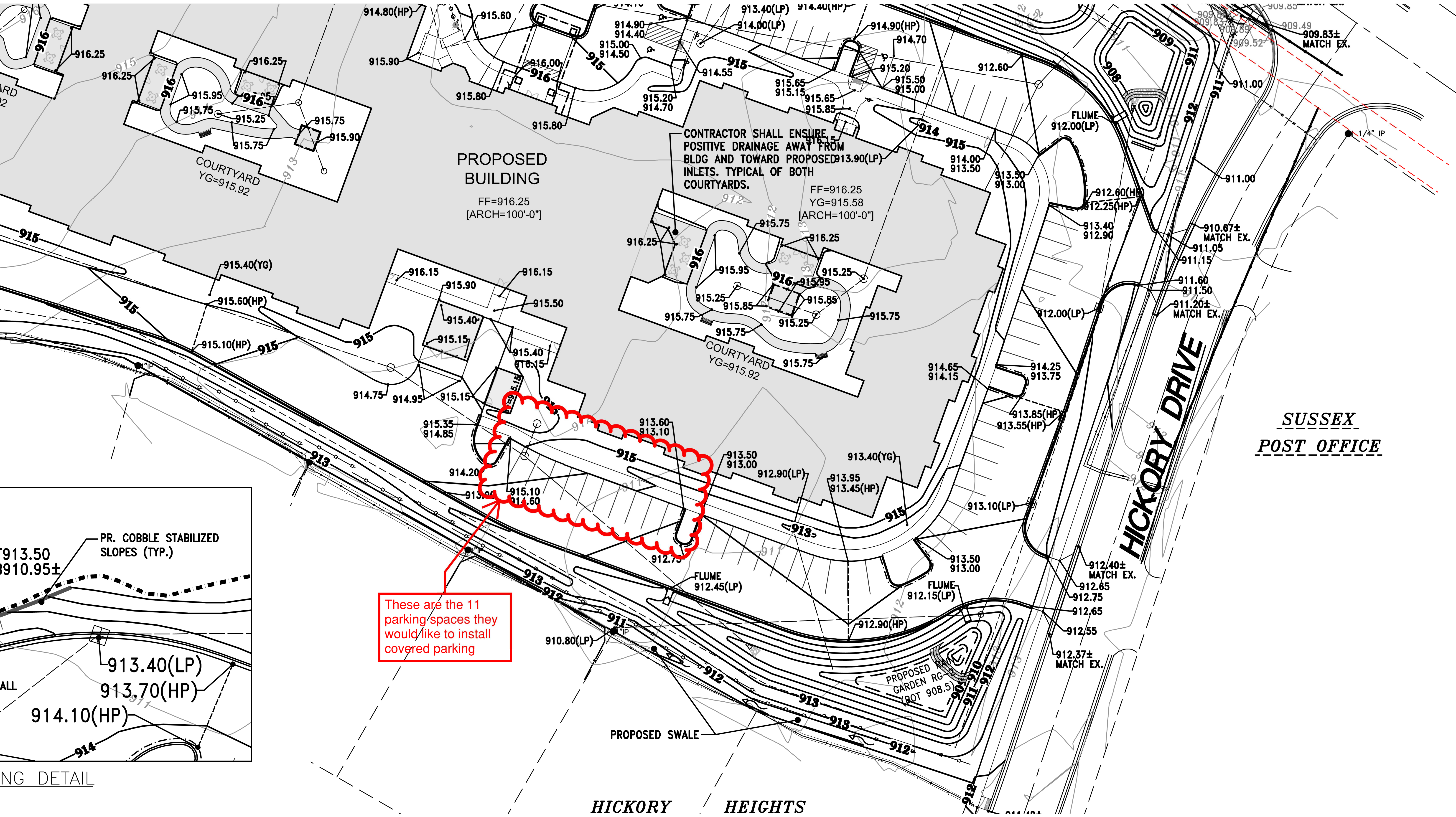
N64 W23760 Main Street  
Sussex, WI 53089  
262-246-5200

Print Date: 6/29/2021

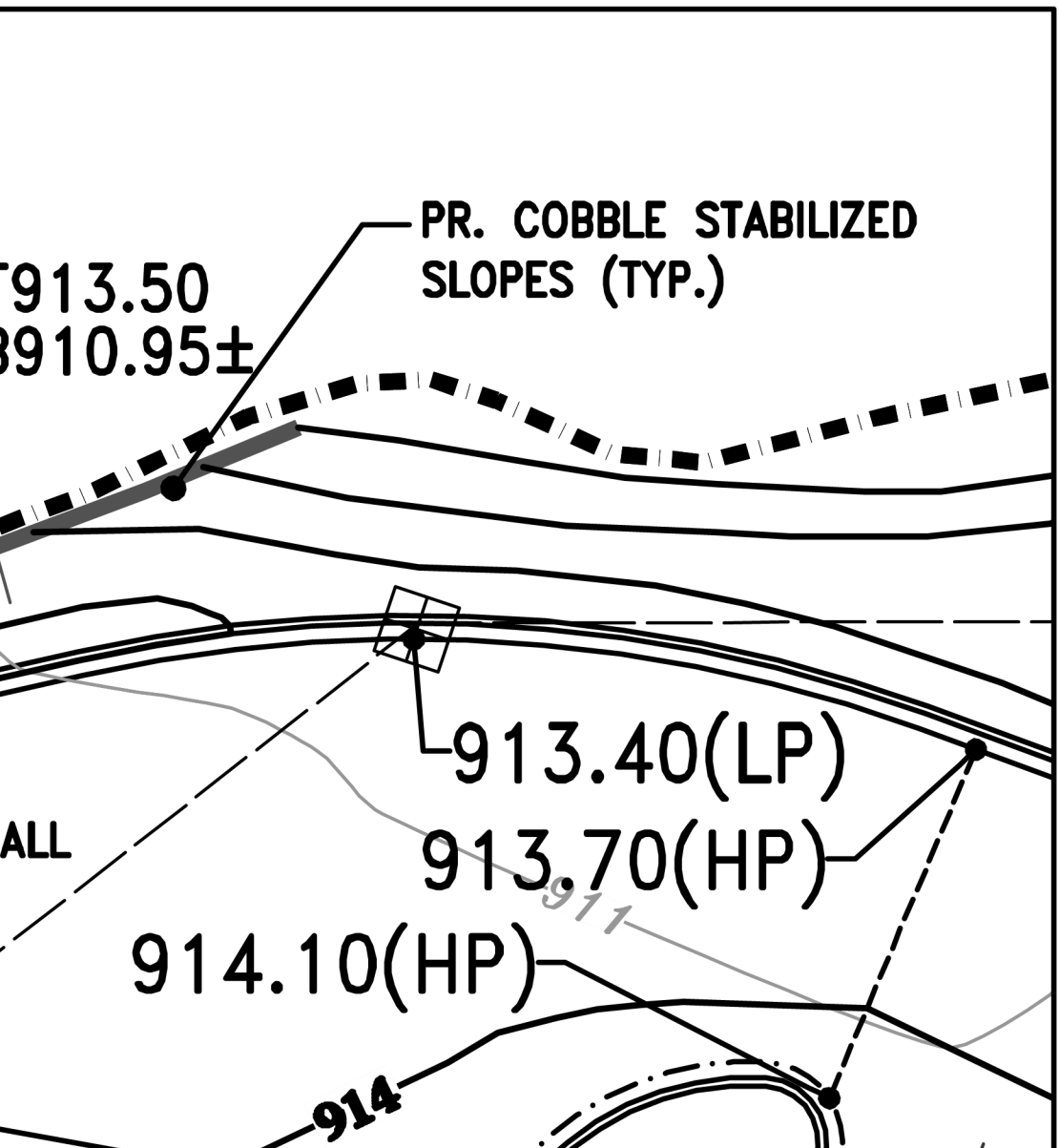








These are the 11 parking spaces they would like to install covered parking



NG DETAIL





## PLAN OF OPERATION

To be used for a business with changes or new business in an existing building.

Is this request to be considered for a Conditional Use? \_\_\_\_\_ If yes, is this a new CU? \_\_\_\_\_

(Conditional Use Permits require a Public Hearing)

OR an amendment to a CU? \_\_\_\_\_

Tax Key # \_\_\_\_\_

Zoning: \_\_\_\_\_

Address of Tenant Space: N62 W23456 Silver Spring Dr.

1. Name of Business:

Sussex Convenience INC

Business

N62 W23456 Silver Spring Dr. 920-809-2600

Address

City, State, Zip

Phone #

Fax #

Email address

Duconvenience@gmail.com

2. Business owner contact information:

Contact

Bishnu P. Adhikari

Address

City, State, Zip

Phone #

N62 W23456

Silver Spring

920-809-2600

Fax #

Email address

3. Building/Land owner contact information:

Contact

BISON ESTATE LLC, Bishnu Adhikari

Address

City, State, Zip

Phone #

N62 W23456

Silver Spring

920-809-2600

Fax #

Email address

4. Number of Employees/Shifts:

2

Employees

2

Shifts

5. Days of Operation:

Put an X in box that applies:

Hours

Open for business

Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
X	X	X	X	X	X	X
5:00-9pm	5am-9pm	5am-9pm	5am-9pm	5am-9pm	6am-9pm	7am-9pm

6. Is this an extension of an existing operation? \_\_\_\_\_
7. On a separate sheet of paper explain your business use and/or the change to your business. Attach a list of all items to be sold, produced or stored on the premises.
8. Is a liquor license or any other special license to be obtained from the Village Board or State Licensing Agencies? yes Do you need an Outdoor Establishment Permit? No  
If yes, explain: from the village of Sussex  
If yes, please obtain and complete permit application.
9. If your proposed operation will utilize a liquor license, what types of entertainment are you proposing? Convenience Store.
10. Do you feel there will be any problems such as odor, smoke or noise resulting from this operation? NO If yes, explain: \_\_\_\_\_
11. Dimension of area to be occupied \_\_\_\_\_ Total square footage \_\_\_\_\_  
If applicable list square footage according to 1<sup>st</sup> floor \_\_\_\_\_ 2<sup>nd</sup> floor \_\_\_\_\_

Please provide a copy of the site plan showing parking and layout of the building and if a multi tenant building, label the space which will be occupied by your business.

12. Parking:  
Total Number of Parking Spaces \_\_\_\_\_ Number of spaces needed per code \_\_\_\_\_  
Number of spaces allocated for employee parking \_\_\_\_\_  
Dimensions of parking lot \_\_\_\_\_ Is parking lot paved? \_\_\_\_\_

13. Signage: What type of signage are you proposing for your business?

Re-using existing signage.  
If applicable, complete a Sign Permit application and submit to the Building Inspection Dept. Please refer to Chapter 17.0800.

I have reviewed a copy of the Zoning Ordinance in the Village of Sussex and hereby agree to abide by the same, as well as any and all terms and conditions of any permit issued pursuant to this application.

I do swear to or affirm that all statements contained herein are true and correct to the best of our knowledge.

Bishnu P. Adhikari 4.30.21  
Name Date  
owner / president  
Title or Position

I am aware and approve of the business to be operating in the building owned by BISON ESTATE LLC.

Bishnu Adhikari 4.30.2021  
Name Date  
owner / member  
Title or Position

## Sussex Convenience

### Use:

- Continue existing Gas-Station Convenience store operation
- Add Beer and Liquor into the product mix

### Products

- Gasoline
- Cigarette, Tobacco & Vape
- Soda, water, juice & Energy drinks
- Frozen food
- General Snacks, candy
- Beer and Liquor
- Coffee, Cappuccino
- Ice Cream, Milk, Butter & yogurt
- Uhaul rental and moving boxes
- DOT license plate renewal Center





## Village of Sussex

### DISCLAIMER:

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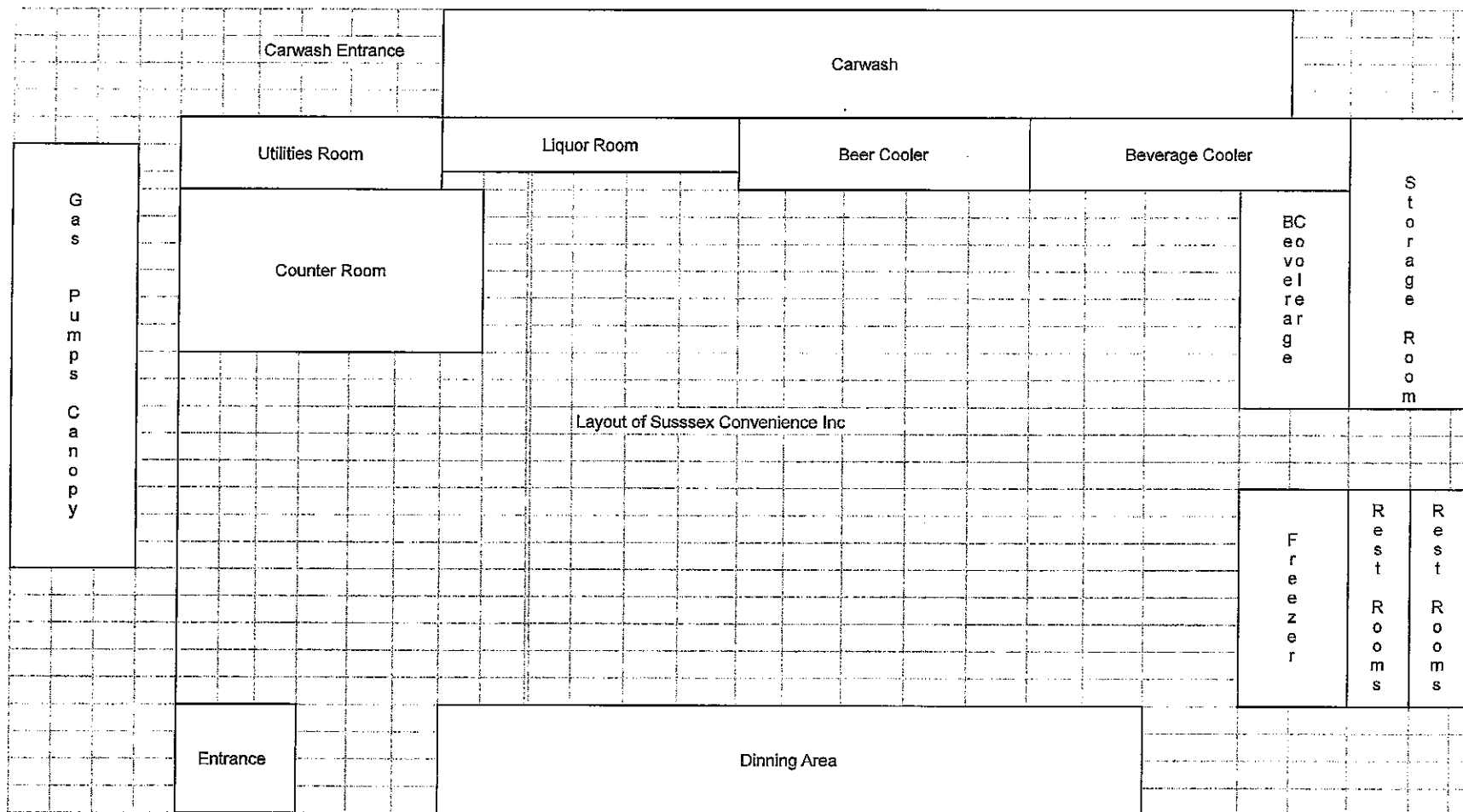


SCALE: 1 = 83'



**Village of Sussex**  
N64 W23760 Main Street  
Sussex, WI 53089  
262-246-5200

Print Date: 5/11/2021



2021  
VILLAGE OF SUSSEX ORDER  
GRANTING A LEGAL NON-CONFORMING CONDITIONAL USE AND  
PRESCRIBING CONDITIONS FOR  
SUSSEX CONVENIENCE INC.  
IN THE VILLAGE OF SUSSEX

WHEREAS, a petition has been filed by Bishnu Adhikari member of Sussex Convenience INC. (hereinafter “PETITIONER”); and

WHEREAS, the petitioner is requesting that a legal non-conditional use permit be granted pursuant to the Zoning Ordinance for the Village of Sussex for property commonly referred to as parcel number SUXV0245938, N62W23456 Silver Spring Sussex, Wisconsin, all as described on Exhibit A attached hereto and incorporated herein (hereinafter “SUBJECT PROPERTY”); and

WHEREAS, the subject property is zoned B-4 Central Mixed Use District; and

WHEREAS, a vehicle washing and gasoline service station in the B-4 Central Mixed Use District is a legal non-conforming use in accordance with Section 17.0507 (A) of the Village of Sussex Zoning Ordinance; and

WHEREAS, pursuant to Section 17.0507 (A) of the Village of Sussex Zoning Ordinance Legal Non-Conforming Uses may be granted conditional use status upon petition of the owner where such use is determined to not be any of the following:

1. Adverse to any of the following:
  - (a) public health,
  - (b) safety, or
  - (c) welfare;
2. In conflict of the spirit or intent of this chapter; or
3. Otherwise detrimental to the community and particularly the surrounding neighborhood.

WHEREAS, pursuant of the Village of Sussex Zoning Ordinance the Plan Commission finds that the gasoline service station shall not cause traffic congestion, as adequate traffic control measures have been taken to ameliorate the traffic concerns; that lighting and glare shall not extend into adjacent residential neighborhoods; that service islands shall comply with the minimum setback requirements of the B-4 Central Mixed Use District; and that canopies over the gasoline service islands encroach no more than six feet into any required yard, and do not encroach into a street right-of-way; and

WHEREAS, pursuant to Section 17.0506(A)(11)(a) all outside storage is a conditional use and location of items and screening will remain the same.

WHEREAS, upon receipt of the application for a legal non-conforming conditional use permit filed by the petitioner, the Village Clerk properly referred such petition to the Plan Commission for the Village of Sussex for determination; and

WHEREAS, upon referral of the petition by the Village Clerk, the Plan Commission for the Village of Sussex scheduled a public hearing thereon as soon as practical, and



WHEREAS, upon publication of the required “Notice of Public Hearing” and mailing of said “Notice of Public Hearing” to all parties-in-interest as required by Section 17.1401 of the Zoning Ordinance, the Plan Commission held a public hearing on June 15, 2021 and July 20, 2021 as required by Section 17.0505 of the Zoning Ordinance for the Village of Sussex; and

WHEREAS, the Plan Commission for the Village of Sussex, following the public hearings and necessary study and investigation, having given the matter due consideration, and having based its determination on the effect of granting such legal non-conforming conditional use permit on the health, general welfare, safety, and economic prosperity of the Village and specifically of the immediate neighborhood in which said use will be located, and having given due consideration to the municipal problems involved, as well as impact on the surrounding properties as to noise, dust, smoke, odor, or other similar factors, hereby determines that the use will not violate the spirit or intent of the Zoning Ordinance for the Village of Sussex, will not be contrary to the public health, safety or general welfare of the Village of Sussex, will not be a hazardous, harmful, noxious, offensive, or nuisance by reason of smoke, dust, odor, or other similar factors and will not for any other reason cause a substantial adverse effect on the property values and general desirability of the neighborhood as long as the legal non-conforming conditional use is operated pursuant to the following conditions and in strict compliance with the same.

THEREFORE IT IS ORDERED AS FOLLOWS:

Commencing upon the date hereof, a legal non-conforming conditional use permit for the subject property is hereby granted. The legal non-conforming conditional use permit granted herein shall apply only to the specific use of the Subject Property by Sussex Convenience Inc. for the following specific uses: vehicle washing, outside storage and 24 hour gas station, as well as permitted and accessory uses set forth in Section 17.0419 B-4 Central Mixed Used District and auxiliary uses as set forth in 17.0710 if approved as part of a Plan of Operation and site plan; and the legal non-conforming conditional use permit shall continue in existence only so long as the legal non-conforming conditional use is operated in compliance with this permit. This legal non-conforming conditional use permit is subject to initial and continued compliance with each and every one of the following conditions, restrictions, and limitations.

- A. This legal non-conforming conditional use is granted for Sussex Convenience Inc. for the above enumerated uses, subject to the following conditions:
  1. Presentation Compliance. The vehicle washing and 24-hour gas station is approved herein on the Subject Property, and the use thereof, shall be used in substantial conformity with the presentation at the Public Hearings before the Plan Commission held on June 15, 2021 and July 20, 2021.
  2. Subject Property. This legal non-conforming conditional use permit issued to the Petitioner, shall be limited to the property described in Exhibit “A” attached hereto and incorporated herein as presented at the public hearing.
  3. Plans. The Petitioner is required and must have all plans current, approved by the Plan Commission for the Village of Sussex, and on file with the Plan Commission for the Village of Sussex in order for this LNCU to be in effect. The Petitioner shall be entitled to amend or change any plan contemplated herein subject to the aforementioned conditions and subject to the Plan Commission for the Village of Sussex approval and without a public hearing, if such amendments and/or change is not a substantial amendment or change in any plan contemplated

herein that the Plan Commission for the Village of Sussex feels, in its sole discretion, to be substantial will require a new permit and all Village procedures in place at the time must be followed.

A. Site Plan. The Petitioner shall maintain the existing site plan, which shall be attached hereto and incorporated herein as Exhibit B.

B. Plan of Operation. The Petitioner shall submit to and receive approval from the Village Plan Commission a specific plan of operation for the Subject Property, the same to be in substantial conformance with the preliminary plans on file and with the presentation at the meeting. Once approved the plan of operation shall be attached hereto and incorporated herein as Exhibit C.

1. The hours of operation for the gas pumps will be 24 hours. While it is anticipated there will be no issues with the 24-hour operation this provision is specifically highlighted for reconsideration by the Plan Commission should concerns or problems arise related to the extended hours of operation.
2. The Gas station may apply for a license to sell beer, wine, and or liquor and may sell said products, if approval by the Village is granted only within strict compliance of said license and the conditions of approval of the same, and with compliance to any amendments to said license(s) made over time.

C. Traffic, Access and Loading Plans. The petitioner shall comply with the specific traffic, access, loading, and egress plan on file with the Village Clerk.

D. Lighting Plan. The petitioner shall adhere to the lighting plan on file for the subject property. No light rays shall be directed onto neighboring properties.

E. Signage Plan. All signage on the subject property shall conform to the requirements set forth in Section 17.0800 of the Village Zoning Ordinance and the Design Standards of the Village of Sussex, except the two existing pole signs which may exist with what rights they may have as legal nonconforming signs. No change in signage shall be permitted until approved by the Architectural Review Board and Building Inspector for the Village of Sussex.

1. The site has two existing pole signs that may have legal nonconforming rights. The sign face may be changed on these pole signs without a public hearing. If either or both of these pole sign structures need to be replaced or modified the signage plan for the entire site must come into compliance with the then current sign code as a condition of granting this legal non-conforming conditional use. Compliance with this standard if modification or replacement is sought may and is likely to require the removal of the pole signs in their entirety.

F. Sewer, Water, Stormwater and Erosion Control Plans. The Petitioner shall comply with the sewer, water, stormwater and erosion control plans on file with the Village Clerk.

G. Outdoor Storage Plan. No outdoor storage shall be permitted on the subject property with the following specific exceptions:

1. Trash dumpsters limited to the minimum number needed to meet the recycling law requirements. The trash dumpsters shall be screened from view and shall be maintained in a safe and sanitary condition at all times.
  2. The storage and sale of propane tank in the style, manner and location of the outdoor storage plan on file with the Village Clerk.
  3. No other outside storage or sales of merchandise from the above listed materials (excluding gasoline for vehicle usage) are permitted outside of the building.
  4. A limit of ten parking stalls for the storage/rental of Uhaul trucks and trailers.
4. Adult-Oriented Materials. No adult-oriented materials or pornographic videotapes, magazines, or gift items will be sold or rented from the subject facility.
  5. Licenses. The Petitioner shall be required to obtain any and all required licenses and permits from the Village, County, State, and Federal Government. If any license or permit is issued, any and all conditions of the same are incorporated herein and made a part of this legal non-conforming conditional use permit.
  6. Laws. The Petitioner shall comply with all Federal, State, County, and local rules, codes, ordinances, and regulations in the construction, operation, and maintenance of the Subject Property.
  7. Building and Fire Inspection. The Petitioner is required to keep the Subject Property in compliance with all federal, State, and local laws, statutes, codes, ordinances, policies, and guidelines as determined by the Building and Fire Inspectors of the Village of Sussex.
  8. Aesthetics. The Petitioner is required to properly maintain the Subject Property at all times and in full compliance with the Village Property Maintenance Code, Chapter 9 of the Village of Sussex Code all to the satisfaction of the Plan Commission for the Village of Sussex.
  9. Junk. No junk shall be accumulated or stored on the Subject Property. No burying or burning of junk is permitted on the Subject Property.
  10. Temporary Use or Activity. No temporary use or special activity or event shall be permitted without prior approval of the Plan Commission of the Village of Sussex and the same must be in compliance with all ordinance, rules, and regulations of the Village of Sussex and all necessary permits must be obtained.
  11. Fees and Expenses. The Petitioner, upon issuance of this legal non-conforming conditional use permit, shall reimburse the Village of Sussex for all expenses incurred by the Village, including, but not limited to, expenses for the Village Administrator, Village Engineer, Village Attorney, and all other professionals and technical assistance realized by the Village in approving and granting this legal non-conforming conditional use permit. The Village Clerk shall provide the Petitioner with copies of all itemized invoices.
  12. Enforcement. Any attorney fees incurred by the Village of Sussex to enforce any of the conditions or requirements of this legal non-conforming conditional use permit must be paid by the Petitioner.

13. Complaints. In the event the Petitioner receives any complaints with regard to the operation authorized by this legal non-conforming conditional use, the Petitioner shall respond to such complaints in writing within a reasonable time not to exceed two weeks from the date of the complaint and shall provide a copy of the written response to the Sussex Village Administrator within the same period of time. If the complaint was made in writing, the copy provided to the Village Administrator shall include a copy of the complaint.
  14. No Nuisance. The Village reserves the right to rescind its approval of this legal non-conforming conditional use permit based upon the finding that the use is incompatible and a nuisance to surrounding uses, that the use is not in the public interest, or that the use adversely affects the use of adjacent lands, provided the Petitioner is given an opportunity to be heard on the matter and, if so rescinded the Petitioner and Subject Property shall thereupon be immediately subject to the Village of Sussex Zoning Ordinances, as applicable, regarding the use of the Subject Property as though no legal non-conforming conditional use permit was granted.
  15. Subject to Acceptance. Subject to the Owner approving in writing the issuance of the same and Petitioner acknowledging in writing that they have received a copy of this legal non-conforming conditional approval, that they understand and accept the same, and that upon failure to satisfy these conditions this approval is void, and the same is deemed to not have been approved, and the Petitioner will therefore need to re-commence the application process.
  16. Review. The Plan Commission for the Village of Sussex reserves its right to review the operation and amend the legal non-conforming conditional use permit as the Plan Commission for the Village of Sussex deems appropriate
- B. Any use not specifically listed as permitted shall be considered to be prohibited except as may be otherwise specifically provided herein. In case of a question as to the classification of use, the question shall be submitted to the Plan Commission for the Village of Sussex for determination.
  - C. No use is hereby authorized unless the use is conducted in a lawful, orderly and peaceful manner. Nothing in this order shall be deemed to authorize any public or private nuisance or to constitute a waiver, exemption or exception to any law, ordinance, order or rule of either the Village of Sussex, the County of Waukesha, the State of Wisconsin, the United States of America or other duly constituted authority, except only to the extent that it authorizes the use of the subject property above described in any specific respects described herein. This order shall not be deemed to constitute a building permit, nor shall this order constitute any other license or permit required by Village ordinance or other law.
  - D. This legal non-conforming conditional use hereby authorized shall be confined to the subject property described, without extension or expansion other than as noted herein, and shall not vary from the purposes herein mentioned unless expressly authorized in writing by the Plan Commission for the Village of Sussex as being in compliance with all pertinent ordinances.
  - E. Should the permitted legal non-conforming conditional use be abandoned in any manner, or discontinued in use for twelve (12) months, or continued other than in strict conformity with the conditions of the original approval, or should the petitioner be delinquent in payment of any monies due and owing to the municipality, or should a change in the character of the surrounding area or the use itself cause it to be no longer compatible with the surrounding area

or for similar cause based upon consideration of public health, safety or welfare, the legal non-conforming conditional use may be terminated by action of the Plan Commission for the Village of Sussex.

- F. Any change, addition, modification, alteration and/or amendment of any aspect of this legal non-conforming conditional use, including but not limited to a change in the use, premises, lands or owners, other than as specifically authorized herein, shall require a new permit and all Village procedures in place at the time must be followed.
- G. Unless this legal non-conforming conditional use permit expressly states otherwise, plans that are specifically required by this legal non-conforming conditional use order may be amended upon the prior approval of the Plan Commission if the Plan Commission finds the plan amendment to be minor and consistent with the legal non-conforming conditional use permit. Any change in any plan that the Plan Commission, in its sole discretion, finds to be substantial shall require a new permit, and all procedures in place at the time must be followed.
- H. Should any paragraph or phrase of this legal non-conforming conditional use permit be determined by a Court to be unlawful, illegal or unconstitutional, said determination as to the particular phrase or paragraph shall not void the rest of the legal non-conforming conditional use and the remainder shall continue in full force and effect.
- I. This legal non-conforming conditional use permit shall be effective for an initial term that ends five years from the date of January 1 of the year the legal non-conforming conditional use is approved. During the fifth year of the LNCU the Village Administrator or designee shall examine the record to determine if concerns about the operation have been raised in writing by the public, a governmental body or official, or anyone else, and shall also review the site and the adjoining area for compliance with the conditions of the LNCU. If based upon that review the use of the property is compatible with the surrounding areas and the petitioner is in substantial compliance with all terms of this legal non-conforming conditional use agreement, then, in that event, the legal non-conforming conditional use can be automatically renewed for another 5-year term. If based upon that review the Village Administrator finds concerns about compliance with the conditions of the legal non-conforming conditional use, the petitioner shall be brought back before the Plan Commission for consideration of the renewal. The Petitioner shall have the responsibility to apply for the renewal by January 30<sup>th</sup> of the fifth (5) year of the term. Failure of Petitioner to apply for the renewal as provided herein shall be deemed a violation of the conditions of the legal non-conforming conditional use and may serve as a basis for termination of the legal non-conforming conditional use permit. The Plan Commission for the Village of Sussex may add additional conditions at any time.
  - 1. Where the changing character of the surrounding area causes the original legal non-conforming conditional use or subsequent approved amendments thereto to no longer be compatible with the surrounding area, or for similar cause, based upon consideration for the public welfare, the legal non-conforming conditional use order and any subsequent approved amendments or changes may be terminated by action of the Plan Commission of the Village of Sussex. Such use shall thereafter be classified as a legal nonconforming use as it was permitted to exist on the day it was terminated.
  - 2. Where this permitted legal non-conforming conditional use does not continue in conformity with the conditions of the original approval or subsequent approved amendments or changes, the legal non-conforming conditional use grant and any



subsequent approved amendments thereto may be amended or terminated by action of the Plan Commission for the Village of Sussex. The Plan Commission for the Village of Sussex may require complete termination of such use.

3. This legal non-conforming conditional use may be reviewed annually. Additionally, this conditional use may be reviewed by the Plan Commission for the Village of Sussex at any time upon complaint or upon Plan Commission initiative.

- J. Upon acceptance by petitioner of this legal non-conforming conditional use permit, all prior legal non-conforming conditional use permits granted to the subject property are hereby revoked and terminated.
- K. If any aspect of this legal non-conforming conditional use permit or any aspect of any plan contemplated and approved under this legal non-conforming conditional use is in conflict with any other aspect of the legal non-conforming conditional use or any aspect of any plan of the legal non-conforming conditional use, the more restrictive provision shall be controlling as determined by the Plan Commission for the Village of Sussex.
- L. If any paragraph or phrase of this legal non-conforming conditional use order is declared by a Court of competent jurisdiction to be invalid, unlawful or unenforceable, such decision shall apply only to the specific paragraph or phrase thereof directly specified in the decision and shall not affect the validity of any other provisions, sections or portions thereof of this order. The remainder of the order shall remain in full force and effect.

Let copies of this order be filed in the permanent records of the Plan Commission for the Village of Sussex, and let copies be sent to the proper Village authorities and the petitioner.

Approved this \_\_\_\_\_ day of \_\_\_\_\_, 2021  
BY THE PLAN COMMISSION FOR THE VILLAGE OF SUSSEX

\_\_\_\_\_  
Anthony LeDonne  
Village President

\_\_\_\_\_  
Samuel Liebert  
Village Clerk

#### ACCEPTANCE

I hereby accept the terms of this legal non-conforming conditional use in its entirety.

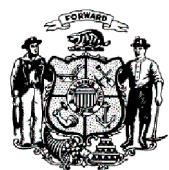
Dated this \_\_\_\_\_ day of \_\_\_\_\_, 2021

By: \_\_\_\_\_

This legal non-conforming conditional use was drafted by Village Administrator Jeremy Smith based off of a model from previous legal non-conforming conditional uses for the subject property that were drafted by Attorney John P. Macy ARENZ, MOLTER, MACY & RIFFLE, S.C.  
Village Attorneys for the Village of Sussex

There are no objections to this plat with respect to Secs. 236.15, 236.16, 236.20 and 236.21(1) and (2), Wis Stats. as provided by s. 236.12, Wis. Stats.

Certified \_\_\_\_\_, 20\_\_\_\_



Department of Administration

**EXISTING ZONING:**  
RS-3 (PDO)

**DESIGN CRITERIA:**  
MIN. FRONT SETBACK: 30 FT.  
MIN. REAR SETBACK: 25 FT.  
MIN. SIDE SETBACK: 10 FT.

# SUSSEX PRESERVE ADDITION NO. 4

BEING A SUBDIVISION OF A PART OF LOT 2, CERTIFIED SURVEY MAP NO. 7984, AND A PART OF THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 27 AND THE NORTHEAST 1/4 AND THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 34, ALL IN TOWN 8 NORTH, RANGE 19 EAST, IN THE VILLAGE OF SUSSEX, WAUKESHA COUNTY, WISCONSIN.

## OWNER:

MAPLE GROVE PRESERVE, LLC  
c/o ARTHUR SAWALL  
4375 PILGRIM HOLLOW COURT  
BROOKFIELD, WI 53005  
262-893-4848

## GENERAL NOTES:

- - Indicates Set 1.270" outside diameter x 18" long Reinforcing Bar weighing 4.303 lbs. per lineal foot. All other Lot and Outlot corners are staked with 0.750" outside diameter x 18" long Reinforcing Bar weighing 1.502 lbs. per lineal foot.
- - Indicates Found 0.75" Reinforcing Bar, (Unless stated otherwise).

- All linear measurements have been made to the nearest one-hundredth of a foot.

- All angular measurements have been made to nearest second and computed to the nearest half-second.

- All bearings are referenced to Grid North of the Wisconsin State Plane Coordinate System, South Zone (NAD-27), in which the North line of the N.E. 1/4 of Section 34, Town 8 North, Range 19 East, bears North 89°31'37" East.

- The Landscape Island Easement as shown on the "Sussex Preserve" Subdivision with Landscaping herein granted to each individual Lot Owner within this Subdivision and each individual Lot Owner within any future addition to this Subdivision. The Owners of the residential Lots within this Subdivision and the Owners of any Lots within any future addition to this Subdivision shall each be liable for an equal undivided fractional share of the cost to repair, maintain or restore the Entrance Monument Signage and Landscaping within Outlot 3 of "Sussex Preserve" and said Landscape Island Easement. Said repairs, maintenance and restoration shall be performed by the Owners of all Lots within this Subdivision and the Owners of any Lots within any future addition to this Subdivision.

- D** The Landscape Island Easement as shown on "Sabrina Court" with Landscaping has been granted to each individual Lot Owner within this Subdivision and each individual Lot Owner within any future addition to this Subdivision. The Owners of the residential Lots within this Subdivision and the Owners of any Lots within any future addition to this Subdivision shall each be liable for an equal undivided fractional share of the cost to repair, maintain or restore. Said repairs, maintenance and restoration shall be performed by the Owners of all Lots within this Subdivision and the Owners of any Lots within any future addition to this Subdivision.

- Outlots 10, 11, 12, 13, 14, and 16 are to be retained by the developer for future development.

- Outlot 15 is hereon dedicated to the Village of Sussex.

- The 100 year Floodplain line, as shown on FEMA flood Map Number 55133C0201G, dated November 4th, 2014, encroaches into Outlots 14 and 15 and the Right-of-Way of "Peppertree Drive South". The 100 year Floodplain line as shown on this plat has been updated and approved by CLOMR Case Number 19-05-3007R on this plat. Per CLOMR Case Number 19-05-3007R all lots are now located within Zone "X".

- Wetland boundaries shown hereon were field delineated by Brian Lennie of Stantec Consulting, Inc. (a WDNR certified assured delineator) on August 25, 2014.

- Any land below the Ordinary High Water Mark of a Lake or a Navigable Stream is subject to the public trust in Navigable Waters that is established under Article IX, Section 1, of the State Constitution.

## BASEMENT RESTRICTION - BEDROCK NOTE:

Although all Lots in the Subdivision have been reviewed and approved for development with single-family residential use in accordance with Section 236 Wisconsin Statutes, some Lots contain soil conditions which, due to the possible presence of bedrock near the ground surface, may require additional soil engineering and foundation design with regard to basement construction. It is recommended that either a licensed professional engineer or other soils expert be consulted regarding the construction of basements in these areas where bedrock may be present near the ground surface. Soil conditions should be subject to each owner's special investigation prior to construction and no specific representation is made herein.

## BASEMENT RESTRICTION - GROUNDWATER NOTE:

Although all Lots in the Subdivision have been reviewed and approved for development with single-family residential use in accordance with Section 236 Wisconsin Statutes, some Lots contain soil conditions that, due to the possible presence of groundwater near the surface, may require additional soil engineering and foundation design with regard to basement construction. It is recommended that a licensed professional engineer design a basement and foundation that will be suitable to withstand the various problems associated with saturated soil conditions on basement walls or floors or that other special measures be taken. Soil conditions should be subject to each owner's special investigation prior to construction and no specific representation is made herein.

## PUBLIC EASEMENT PROVISIONS:

Permanent non-exclusive easements granted to the Village of Sussex ("Village") upon, within, and beneath the land identified on this final plat as:

- A** - "20' Wide Public Storm Sewer Easement"
- B** - "11' Wide Public Sidewalk Easement"
- C** - "15' Wide Public Access Easement"

- Purpose:** The purpose of these Easements is to:

### Public Storm Sewer Easement:

- Install, operate, maintain, and replace underground storm sewer utility facilities, together with all necessary and appurtenant equipment under and above the ground as deemed necessary by the Village, all to transmit storm water.
- Surface storm water conveyance.

### 11' Wide Public Sidewalk and 15' Wide Public Access Easement:

- Construct, maintain, repair, and replace pedestrian sidewalks in the sidewalk easements and path in the access easements depicted on the final plat.
- The general public shall have the right to traverse the "11' Wide Public Sidewalk Easement" and the "15' Wide Access Easement" area for use as a public sidewalk and path.

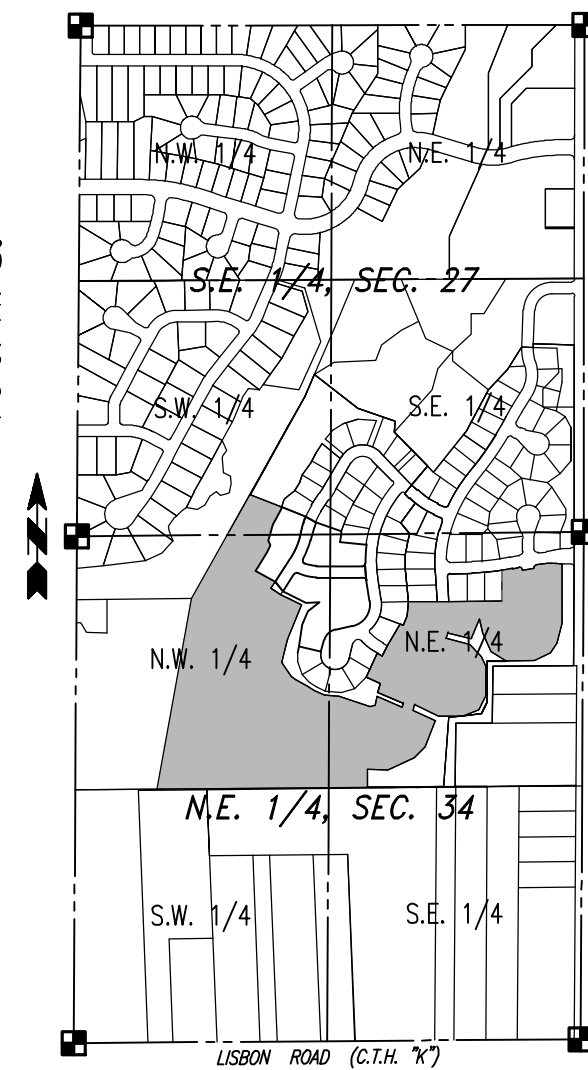
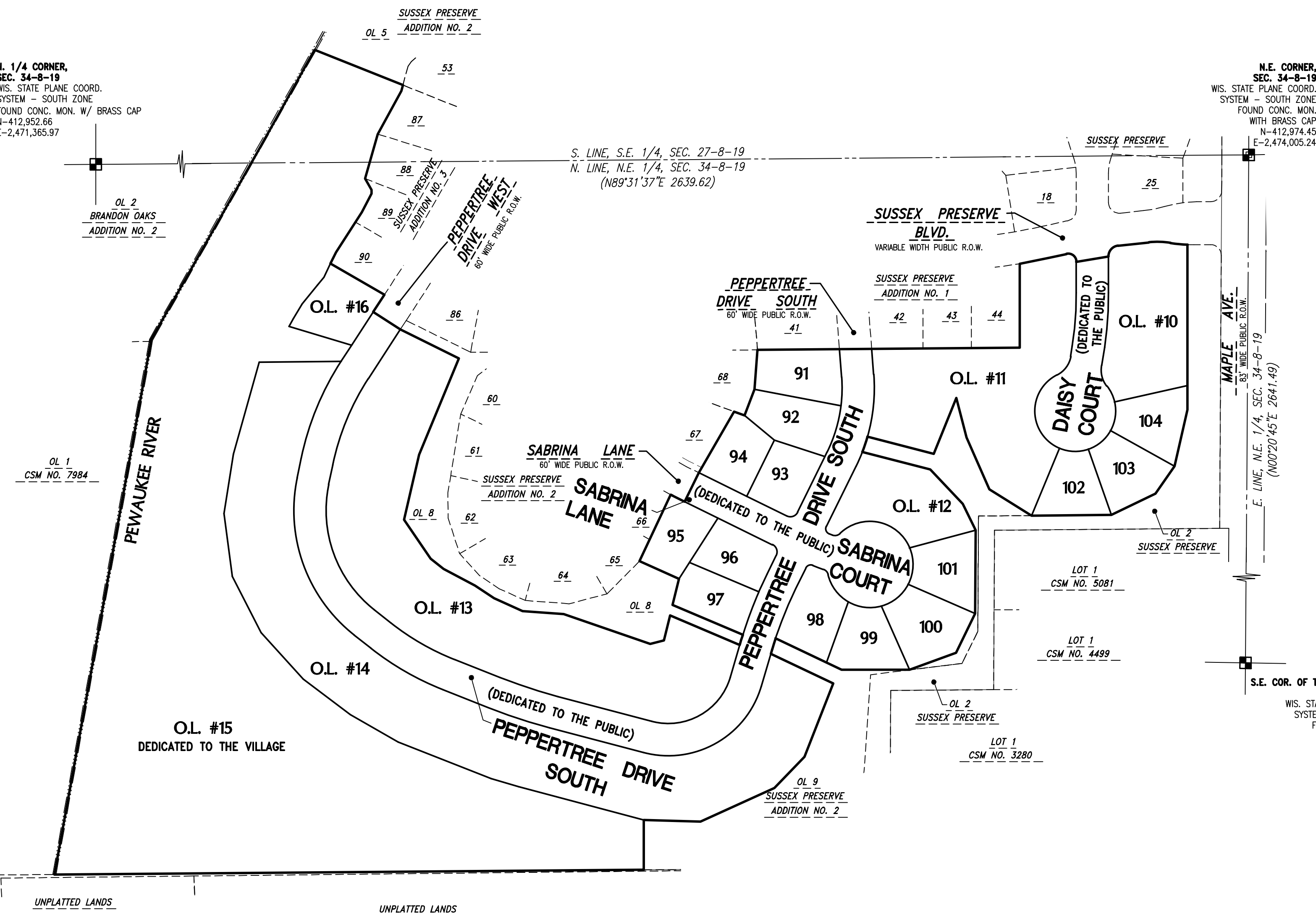
- Buildings and Other Structures.** The land owner agrees that no structures will be erected in the easement area, or in such close proximity to the facilities, such as to prevent the Village from exercising its rights under this easement.

- Elevation.** The land owner agrees that the elevation of the existing ground surface within the easement area will not be altered by more than four (4) inches without the written consent of the Village.

- Restoration.** The Village agrees to restore or cause to have restored the land owner's land to grade and replace sod or grass disturbed. This restoration, however, does not apply to the initial installation or to any trees, bushes, branches, or roots which may interfere with the Village's use of the easement area.

- Exercise of Rights:** It is agreed that the complete exercise of rights herein conveyed may be gradual and not fully exercised until some time in the future, and that none of the rights herein granted shall be lost by non-use.

- These Easement shall run with the land and shall be binding upon and inure to the benefit of and be enforceable by the land owner and the Village and their respective heirs, personal representatives, successors and assigns.



## LOCALITY MAP:

S.E. 1/4, SEC. 27,  
N.E. 1/4, SEC. 34,  
T. 8 N., R. 19 E.  
SCALE: 1"=1000'

S.E. COR. OF THE N.E. CORNER,  
SEC. 34-8-19  
WIS. STATE PLANE COORD.  
SYSTEM - SOUTH ZONE  
FOUND CONC. MON.  
WITH BRASS CAP  
N=410,333.27  
E=2,473,989.30

N.E. CORNER,  
SEC. 34-8-19  
WIS. STATE PLANE COORD.  
SYSTEM - SOUTH ZONE  
FOUND CONC. MON.  
WITH BRASS CAP  
N=412,974.45  
E=2,474,005.24

LOT 1  
CSM NO. 3081

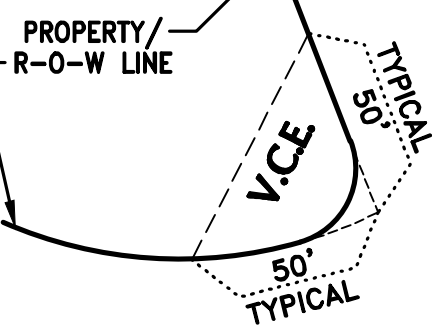
LOT 1  
CSM NO. 4499

LOT 1  
CSM NO. 3280

## VISION CORNER EASEMENT DETAIL (V.C.E.)

### V.C.E. EASEMENT APPLIES TO:

Lots 93, 96, and 98 and Outlots 10, 11, and 12 are herein subject to a Vision Corner Easement as shown on this plat. Vision corner easements are herein granted to the Village of Sussex. No structure of any kind shall be permitted within a vision corner which exceeds 3 feet above the intersection except for necessary highway and traffic signs, public utility lines and open fences, through which there is clear vision, nor shall any plant material be permitted which obscures safe vision of the approaches to the intersection.



4100 N. Calhoun Rd.  
Suite 300  
Brookfield, WI 53005  
Phone: (262) 790-1480  
Fax: (262) 790-1481

SCALE: 1" = 150'  
0 150 300

07-011-974-01

THIS INSTRUMENT WAS DRAFTED BY TED R. INDERMUEHLE, P.L.S. (S-3119)

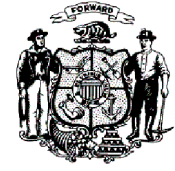
DATED THIS 17TH DAY OF JUNE, 2021

SHEET 1 OF 5



There are no objections to this plat with respect to  
Secs. 236.15, 236.16, 236.20 and 236.21(1) and (2),  
Wis Stats. as provided by s. 236.12, Wis. Stats.

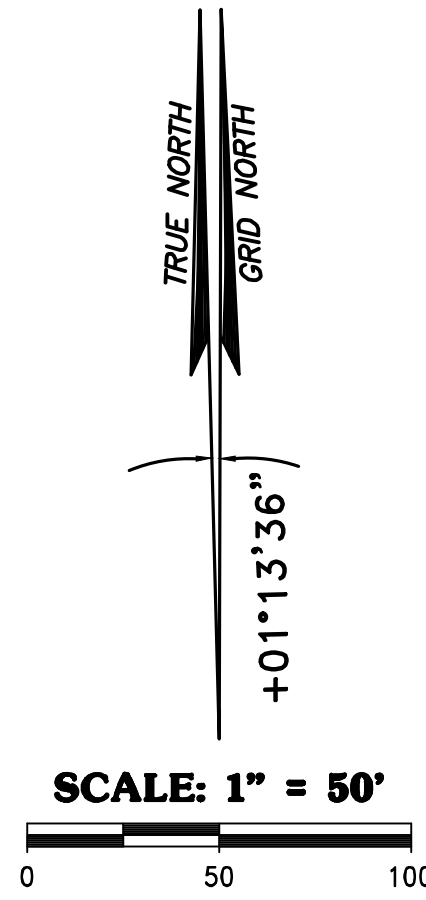
Certified \_\_\_\_\_, 20\_\_\_\_



Department of Administration

# SUSSEX PRESERVE ADDITION NO. 4

BEING A SUBDIVISION OF A PART OF LOT 2, CERTIFIED SURVEY MAP NO.  
7984, AND A PART OF THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF  
SECTION 27 AND THE NORTHEAST 1/4 AND THE NORTHWEST 1/4 OF THE  
NORTHEAST 1/4 OF SECTION 34, ALL IN TOWN 8 NORTH, RANGE 19 EAST,  
IN THE VILLAGE OF SUSSEX, WAUKESHA COUNTY, WISCONSIN.



N.E. CORNER,  
SEC. 34-8-19  
WIS. STATE PLANE COORD.  
SYSTEM - SOUTH ZONE  
FOUND CONC. MON.  
WITH BRASS CAP  
N=412,974.45  
E=2,474,005.24

SUSSEX PRESERVE  
BLVD.  
VARIABLE WIDTH PUBLIC R.O.W.

A=29.30

A=40.19

N83°55'48"E

57.29

A=32.15

N80°25'29"E

60.85

N77°22'12"E

33.36

A=55.20

N89°31'37"E

487.76

N20°52'16"E

51.84

S69°48'28"E

20.63

N26°11'41"E

60.00

S63°48'19"E

25.60

A=3.19

N25°14'19"E

186.41

N66°01'18"W

80.41

N13°40'56"E

49.83

N13°40'56"E

47.70

S66°55'13"E

140.84

N16°28'52"E

9.43

N64°40'42"W

136.09

S64°40'42"E

128.48

N64°40'42"E

126.40

S69°51'57"W

91.71

S92°10'18"W

81.38

S92°07'50"E

54.51

N23°46'50"W

105.42

N14°15'42"W

75.63

N75°36'41"E

130.42

S75°36'41"E

127.57

N79°21'40"W

163.61

N72°02'56"W

98.66

N63°48'19"E

181.88

S63°48'19"E

227.48

N63°48'19"W

181.68

S63°48'19"E

12.39

N66°55'13"W

141.91

S66°55'13"E

140.84

N16°28'52"E

9.43

N64°40'42"W

136.09

S64°40'42"E

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N75°36'41"E

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S69°51'57"W

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S92°10'18"W

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S92°07'50"E

54.51

N23°46'50"W

105.42

N14°15'42"W

75.63

N75°36'41"E

130.42

S75°36'41"E

127.57

N79°21'40"W



# SUSSEX PRESERVE ADDITION NO. 4

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There are no objections to this plat with respect to Secs. 236.15, 236.16, 236.20 and 236.21(1) and (2), Wis Stats. as provided by s. 236.12, Wis. Stats.

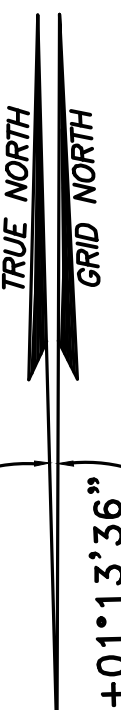
Certified \_\_\_\_\_, 20\_\_\_\_



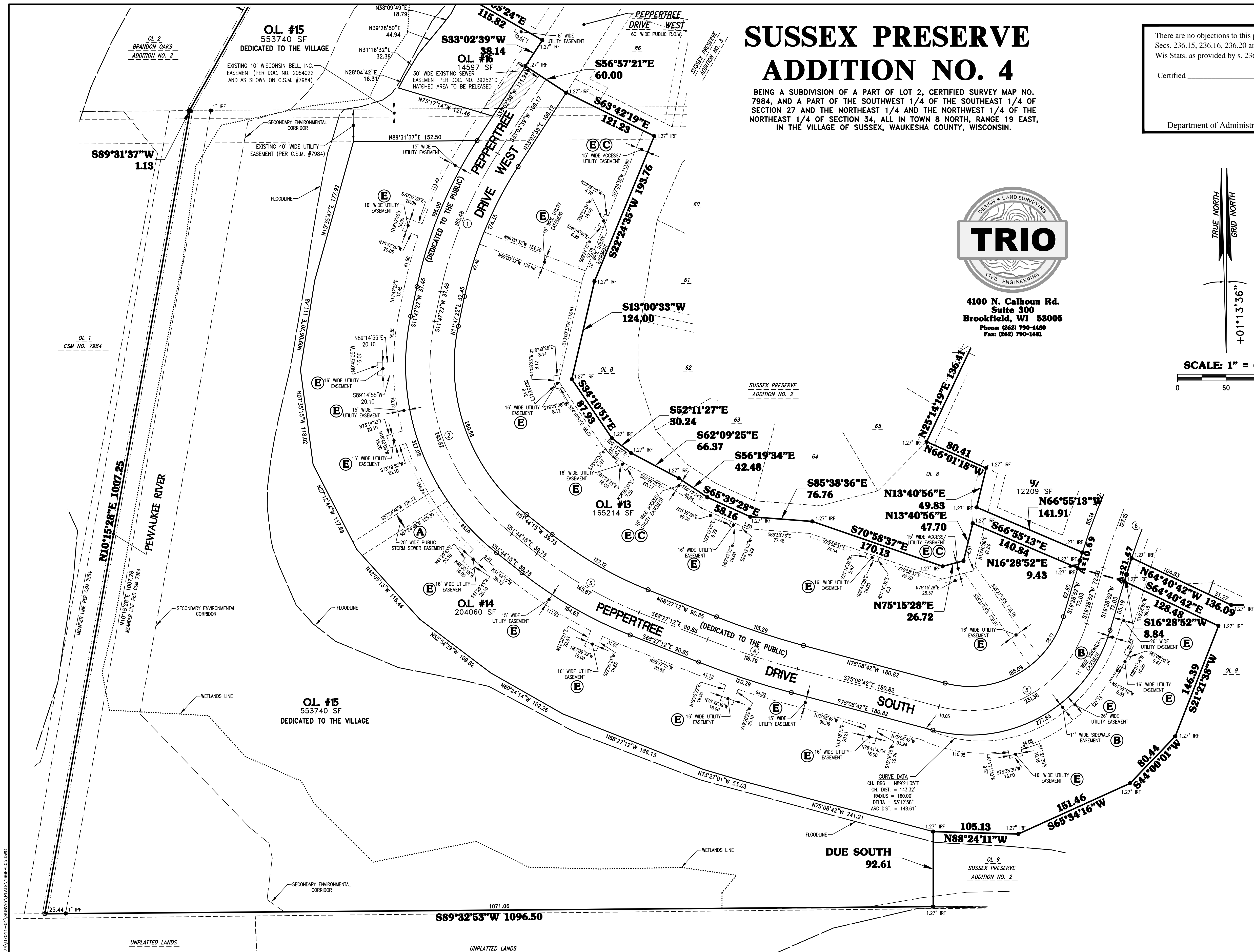
Department of Administration



4100 N. Calhoun Rd.  
Suite 300  
Brookfield, WI 53005  
Phone: (262) 790-1480  
Fax: (262) 790-1481



SCALE: 1" = 60'



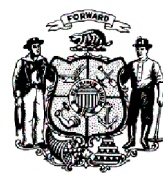


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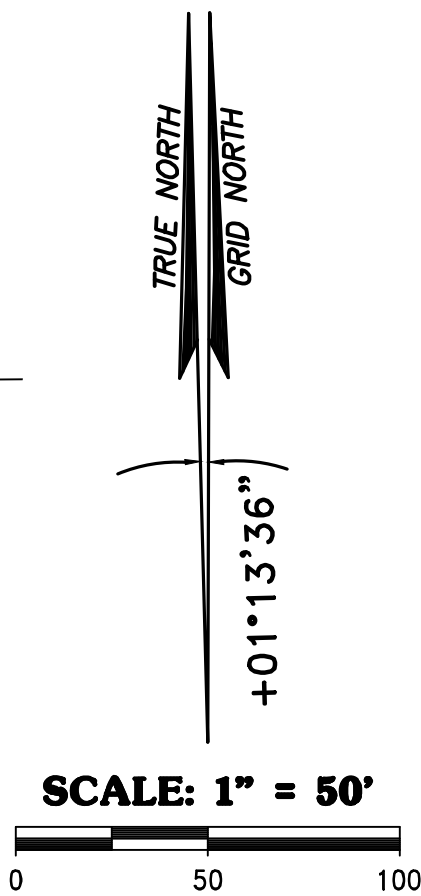
Certified \_\_\_\_\_, 20\_\_\_\_



Department of Administration



4100 N. Calhoun Rd.  
Suite 300  
Brookfield, WI 53005  
Phone: (262) 790-1490  
Fax: (262) 790-1481

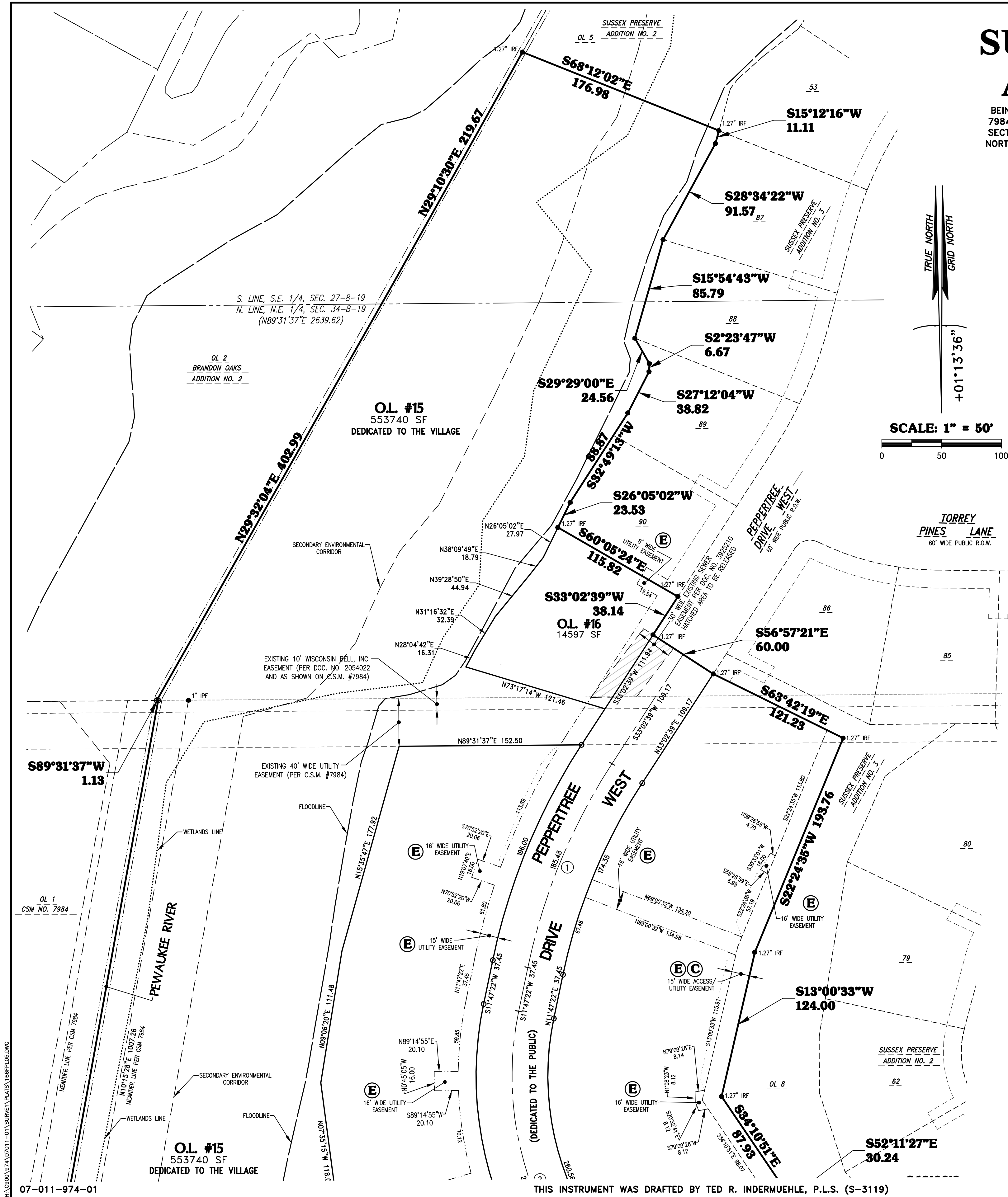


TORREY LANE  
PINES LANE  
60' WIDE PUBLIC R.O.W.

## FLOODPLAIN, WETLANDS, AND SECONDARY ENVIRONMENTAL CORRIDOR PRESERVATION RESTRICTIONS:

Those areas identified as Floodplain, Wetlands, and Secondary Environmental Corridor on this Plat shall be subject to the following restrictions:

- Grading, filling and removal of topsoil or other earthen materials are prohibited except in connection with the construction of a proposed gravel path, unless specifically authorized by the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- The removal or destruction of any vegetative cover, i.e., trees, shrubs, grasses, etc., is prohibited, with the exception that dead, diseased, or dying vegetation may be removed, at the discretion of the landowner and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use-Planning and Zoning Division. Silvicultural thinning upon the recommendation of a forester or naturalist and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use-Planning and Zoning Division shall also be permitted.
- Grazing by domesticated animals, i.e., horses, cows, etc, is prohibited.
- The introduction of plant material not indigenous to the existing environment is prohibited.
- Ponds may be permitted subject to the approval of the Municipality in which they are located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- The construction of buildings is prohibited.



THIS INSTRUMENT WAS DRAFTED BY TED R. INDERMUEHLE, P.L.S. (S-3119)

DATED THIS 17TH DAY OF JUNE, 2021

SHEET 4 OF 5



SURVEYOR'S CERTIFICATE:

STATE OF WISCONSIN )  
COUNTY OF WAUKESHA ) SS

I, Ted R. Indermuehle, Professional Land Surveyor, do hereby certify:

That I have surveyed, divided and mapped a part of lands made up of a Subdivision of a part of Lot 2, Certified Survey Map NO. 7984, and a part of the Southwest 1/4 of the Southeast 1/4 of Section 27 and the Northeast 1/4 of the Northwest 1/4 of the Northeast 1/4 of Section 34, all in Town 8 North, Range 19 East, in the Village of Sussex, Waukesha County, Wisconsin, now being more particularly bounded and described as follows:

Commencing at the Northeast Corner of said Section 34 (also being the Southeast Corner of said Section 27); Thence South 00°20'45" West along the East line of said Northeast 1/4 Section, 162.95 feet to a point; Thence North 89°39'15" West 113.02 feet to a point on the West line of Outlot 2 of "Sussex Preserve" a subdivision of record and the place of beginning of lands hereinafter described;

Thence continuing along the West line of said Outlot 2 the following courses: Due South 306.75 feet to a point; Thence South 12°53'04" West 107.59 feet to a point; Thence South 34°43'29" West 62.36 feet to a point; Thence South 64°23'01" West 91.13 feet to a point; Thence South 89°31'10" West 148.11 feet to a point on the Northerly line of Outlot 9 of "Sussex Preserve Addition No. 2" a subdivision of record; Thence continuing around said Outlot 9 the following courses: North 58°11'44" West 92.74 feet to a point; Thence North 23°46'50" West 105.42 feet to a point; Thence North 14°15'42" West 75.63 feet to a point; Thence South 22°04'33" West 111.93 feet to a point; Thence North 75°36'41" West 127.57 feet to a point; Thence Southerly 30.09 feet along the arc of a curve whose center lies to the West, whose radius is 330.00 feet, whose central angle is 05°13'27" and whose chord bears South 18°32'58" West 30.08 feet to a point; Thence South 75°36'41" East 130.42 feet to a point; Thence South 43°31'38" East 88.12 feet to a point; Thence South 28°07'50" East 54.81 feet to a point; Thence South 00°20'45" West 154.51 feet to a point; Thence South 26°10'18" West 81.38 feet to a point; Thence South 69°51'57" West 91.71 feet to a point; Thence Due West 126.40 feet to a point; Thence North 64°40'42" West 136.03 feet to a point; Thence Southerly 21.47 feet along the arc of a curve whose center lies to the East, whose radius is 720.00 feet, whose central angle is 01°42'32" and whose chord bears South 17°20'08" West 21.47 feet to a point; Thence South 16°28'52" West 8.84 feet to a point; Thence South 64°40'42" East 128.48 feet to a point; Thence South 21°21'38" West 146.39 feet to a point; Thence South 44°00'01" West 80.44 feet to a point; Thence South 65°34'16" West 151.46 feet to a point; Thence North 88°24'11" West 105.13 feet to a point; Thence Due South 92.61 feet to the Southwest corner of said Outlot 9; Thence South 89°32'53" West 1096.50 feet to a point; Thence North 10°15'28" East 1007.25 feet to a point; Thence South 89°31'37" West 1.13 feet to a point; Thence North 29°32'04" East 402.99 feet to a point; Thence North 29°10'30" East 219.67 feet to a point on the South line of Outlot 5 of said "Sussex Preserve Addition No. 2"; Thence South 68°12'02" East along said South line, 176.98 feet to a point on the West line of "Sussex Preserve Addition No. 3" a subdivision of record; Thence South 15°12'16" West 11.11 feet to a point; Thence South 28°34'22" West along said West line, 91.57 feet to a point; Thence South 15°54'43" West along said West line, 85.79 feet to a point; Thence South 29°29'00" East along said West line, 24.56 feet to a point; Thence South 02°23'47" West along said West line, 6.67 feet to a point; Thence South 27°12'04" West along said West line, 38.82 feet to a point; Thence South 32°49'13" West along said West line, 88.87 feet to a point; Thence South 26°05'02" West along said West line, 23.53 feet to a point; Thence South 60°05'24" East along said West line, 115.82 feet to a point on the West Right-of-Way of "Peppertree Drive West"; Thence South 33°02'39" West along said West line, 38.14 feet to a point; Thence South 56°57'21" East 60.00 feet to a point on the boundary of Outlot 8 of said "Sussex Preserve Addition No. 2"; Thence continuing along said Outlot 8 the following courses: South 63°42'19" East 121.23 feet to a point; Thence South 22°24'35" West 193.76 feet to a point; Thence South 13°00'33" West 124.00 feet to a point; Thence South 34°10'51" East 87.93 feet to a point; Thence South 52°11'27" East 30.24 feet to a point; Thence South 62°09'25" East 66.37 feet to a point; Thence South 56°19'34" East 42.48 feet to a point; Thence South 65°39'28" East 58.16 feet to a point; Thence South 85°38'36" East 76.76 feet to a point; Thence South 70°58'37" East 170.13 feet to a point; Thence North 75°15'28" East 26.72 feet to a point; Thence North 13°40'56" East 47.70 feet to a point; Thence South 68°53'13" East 140.84 feet to a point; Thence North 16°28'52" East 9.43 feet to a point; Thence Northerly 10.69 feet along the arc of a curve whose center lies to the East, whose radius is 780.00 feet, whose central angle is 00°47'07" and chord bears North 16°52'25" East 10.69 feet to a point; Thence North 66°55'13" West 141.91 feet to a point; Thence North 13°40'56" East 49.83 feet to a point; Thence North 66°01'18" West 80.41 feet to a point on the East line of "Sussex Preserve Addition No. 2"; Thence North 25°14'19" East along said East line, 136.41 feet to a point; Thence Southeasterly 3.19 feet along the arc of a curve whose center lies to the North, whose radius is 280.00 feet, whose central angle is 00°39'10" and whose chord bears South 63°28'44" East along said East line, 3.19 feet to a point; Thence South 63°48'19" East along said East line, 25.60 feet to a point; Thence North 26°11'41" East along said East line, 60.00 feet to a point; Thence North 29°56'00" East along said East line, 131.04 feet to a point; Thence South 69°48'28" East along said East line, 20.63 feet to a point; Thence North 20°52'16" East along said East line, 51.84 feet to a point; Thence North 04°33'57" East along said East line, 73.74 feet to a point on the South line of "Sussex Preserve Addition NO. 1" a subdivision of record; Thence North 89°31'37" East along said South line, 487.76 feet to a point; Thence Due North along said South line, 156.20 feet to a point on the South Right-of-Way line of "Sussex Preserve Blvd."; Thence Easterly 55.20 feet along the arc of a curve whose center lies to the North, whose radius is 1030.00 feet, whose central angle is 03°04'14" and chord bears North 78°54'19" East along said South line, 55.20 feet to a point; Thence North 77°22'12" East along said South line, 33.36 feet to a point; Thence Southeasterly 26.87 feet along the arc of a curve whose center lies to the Southwest, whose radius is 15.00 feet, whose central angle is 102°37'50" and whose chord bears South 51°18'53" East along said South line, 23.42 feet to a point; Thence North 80°25'29" East along said South line, 60.85 feet to a point; Thence Northeasterly 29.30 feet along the arc of a curve whose center lies to the Southeast, whose central angle is 83°55'48" and whose chord bears North 41°57'54" East along said South line, 28.75 feet to a point; Thence North 83°55'48" East along said South line, 57.29 feet to a point; Thence Easterly 40.19 feet along the arc of a curve whose center lies to the South, whose radius is 150.00 feet, whose central angle is 15°20'59" and whose chord bears South 88°23'42" East along said South line, 40.07 feet to a point; Thence Easterly 32.15 feet along the arc of a curve whose center lies to the North, whose radius is 234.50 feet, whose central angle is 07°51'22" and whose chord bears South 84°38'54" East along said South line, 32.13 feet to the point of beginning of this description.

Said Parcel contains 1,470,313 Square Feet (or 33.7537 Acres) of land, more or less.

That I have made such survey, land division and map by the direction of MAPLE GROVE PRESERVE, LLC, owner of said lands.

That such map is a correct representation of all the exterior boundaries of the land surveyed and the land division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes and the Subdivision Regulations of the Village of Sussex, Waukesha County, Wisconsin in surveying, dividing and mapping the same.

Dated this \_\_\_\_ Day of \_\_\_\_\_, 20 \_\_\_\_.

Ted R. Indermuehle, P.L.S.  
Professional Land Surveyor, S-3119  
TRIO ENGINEERING, LLC  
4100 N. Calhoun Rd. Suite 300  
Brookfield, WI 53005  
Phone: (262)790-1480 Fax: (262)790-1481

UTILITY EASEMENT PROVISIONS:

An easement for electric, natural gas, and communications service is hereby granted by

SANCTUARY AT GOOD HOPE, LLC, Grantor, to

WISCONSIN ELECTRIC POWER COMPANY and WISCONSIN GAS, LLC, Wisconsin corporations doing business as We Energies, Grantee,

WISCONSIN BELL, INC., d/b/a AT&T Wisconsin, a Wisconsin Corporation, Grantee, and

Spectrum Mid-America LLC, Grantee

their respective successors and assigns, to construct, install, operate, repair, maintain and replace from time to time, facilities used in connection with overhead and underground transmission and distribution of electricity and electric energy, natural gas, telephone and cable TV facilities for such purposes as the same is now or may hereafter be used, all in, over, under, across, along and upon the property shown within those areas on the plat designated as 'Utility Easement Areas' and the property designated on the plat for streets and alleys, whether public or private, together with the right to install service connections upon, across within and beneath the surface of each lot to serve improvements, thereon, or on adjacent lots; also the right to trim or cut down trees, brush and roots as may be reasonably required incident to the rights herein given, and the right to enter upon the subdivided property for all such purposes. The Grantees agree to restore or cause to have restored, the property, as nearly as is reasonably possible, to the condition existing prior to such entry by the Grantees or their agents. This restoration, however, does not apply to the initial installation of said underground and/or above ground electric facilities, natural gas facilities, or telephone and cable TV facilities or to any trees, brush or roots which may be removed at any time pursuant to the rights herein granted. Buildings shall not be placed over Grantees' facilities or in, upon or over the property within the lines marked 'Utility Easement Areas' without the prior written consent of Grantees. After installation of any such facilities, the grade of the subdivided property shall not be altered by more than four inches without written consent of grantees.

The grant of easement shall be binding upon and inure to the benefit of the heirs, successors and assigns of all parties hereto.

SUSSEX PRESERVE  
ADDITION NO. 4

BEING A SUBDIVISION OF A PART OF LOT 2, CERTIFIED SURVEY MAP NO. 7984, AND A PART OF THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 27 AND THE NORTHEAST 1/4 AND THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 34, ALL IN TOWN 8 NORTH, RANGE 19 EAST, IN THE VILLAGE OF SUSSEX, WAUKESHA COUNTY, WISCONSIN.

CURVE TABLE:

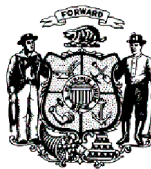
NO.	LOT(S)	RADIUS	CENTRAL ANGLE	ARC	CHORD	CHORD BEARING	TANGENT IN	TANGENT OUT
1	CENTERLINE	500.00	21°15'17"	185.48	184.42	S22°25'00.5"W	S33°02'39"W	S11°47'22"W
	OUTLOT 13	470.00	21°15'17"	174.35	173.35	S22°25'00.5"W	S33°02'39"W	S11°47'22"W
	OUTLOT 14	530.00	21°11'20"	196.00	194.89	S22°23'02"W	S32°58'42"W	S11°47'22"W
2	CENTERLINE	265.00	63°31'37"	293.82	279.00	S19°58'26.5"E	S11°47'22"W	S51°44'15"E
	OUTLOT 13	235.00	63°31'37"	260.56	247.41	S19°58'26.5"E	S11°47'22"W	S51°44'15"E
	OUTLOT 14	295.00	63°31'37"	327.08	310.58	S19°58'26.5"E	S11°47'22"W	S51°44'15"E
3	CENTERLINE	500.00	16°42'57"	145.87	145.36	S60°05'43.5"E	S51°44'15"E	S68°27'12"E
	OUTLOT 13	470.00	16°42'57"	137.12	136.64	S60°05'43.5"E	S51°44'15"E	S68°27'12"E
	OUTLOT 14	530.00	16°42'57"	154.63	154.08	S60°05'43.5"E	S51°44'15"E	S68°27'12"E
4	CENTERLINE	1000.00	6°41'30"	116.79	116.72	S71°47'57"E	S68°27'12"E	S75°08'42"E
	OUTLOT 13	970.00	6°41'30"	113.29	113.22	S71°47'57"E	S68°27'12"E	S75°08'42"E
	OUTLOT 14	1030.00	6°41'30"	120.29	120.22	S71°47'57"E	S68°27'12"E	S75°08'42"E
5	CENTERLINE	150.00	88°22'26"	231.36	209.10	N60°40'05"E	S75°08'42"E	N16°28'52"E
	OUTLOT 13	120.00	88°22'26"	185.09	167.28	N60°40'05"E	S75°08'42"E	N16°28'52"E
	OUTLOT 14	180.00	88°22'26"	277.64	250.92	N60°40'05"E	S75°08'42"E	N16°28'52"E
6	CENTERLINE	750.00	9°42'49"	127.15	127.00	N21°20'16.5"E	N16°28'52"E	N26°11'41"E
	W. R.O.W.	780.00	9°42'49"	132.24	132.08	S21°20'16.5"W	S26°11'41"W	S16°28'52"W
	EXTERIOR	780.00	0°47'07"	10.69	10.69	N16°52'25.5"E	N16°28'52"E	N17°15'59"E
	97	780.00	6°15'14"	85.14	85.09	S20°23'36"W	S23°31'13"W	S17°15'59"W
	96	780.00	2°40'28"	36.41	36.41	S24°51'27"W	S26°11'41"W	S23°31'13"W
	E. R.O.W.	720.00	9°42'49"	122.06	121.92	S21°20'16.5"W	S26°11'41"W	S16°28'52"W
	EXTERIOR	720.00	1°42'32"	21.47	21.47	S17°20'08"W	S18°11'24"W	S16°28'52"W
	98	720.00	8°00'17"	100.59	100.51	S22°11'32.5"W	S26°11'41"W	S18°11'24"W
7	96	15.00	90°00'00"	23.56	21.21	N18°48'19"W	N26°11'41"E	N63°48'19"W
8	98	10.00	85°50'21"	14.98	13.62	S69°06'51.5"W	N67°57'58"W	S26°11'41"W
9	CENTERLINE	500.00	11°35'21"	101.13	100.96	S69°35'59.5"E	S63°48'19"E	S75°23'40"E
	96	533.00	2°17'05"	21.25	21.25	S69°06'30.5"E	S67°57'58"E	S70°15'03"E
	OUTLOT 12	467.00	0°46'32"	6.32	6.32	S69°08'20"E	S68°45'04"E	S69°31'36"E
10	OUTLOT 12	10.00	94°56'45"	16.57	14.74	S21°16'41.5"E	S26°11'41"W	S68°45'04"E
11	OUTLOT 12	13.50	64°21'05"	15.16	14.38	N78°17'51.5"E	S69°31'36"E	N46°07'19"E
12	98	13.50	56°34'33"	13.33	12.80	N41°57'46.5"W	N13°40'30"W	N70°15'03"W
13	93	15.00	90°00'00"	23.56	21.21	N71°11'41"E	S63°48'19"E	N26°11'41"E
14	E. R.O.W.	80.00	300°12'11"	419.16	79.75	N16°13'24.5"E	S13°40'30"E	S46°07'19"W
	98	80.00	46°21'40"	64.73	62.98	S36°51'20"E	S13°40'30"E	S60°02'10"E
	99	80.00	45°30'41"	63.55	61.89	S82°47'30.5"E	S60°02'10"E	N74°27'09"E
	100	80.00	47°19'49"	66.09	64.22	N50°47'14.5"E	N74°27'09"E	N27°07'20"E
	101	80.00	50°01'19"	69.85	67.65	N02°06'40.5"E	N27°07'20"E	N22°53'59"W
	OUTLOT 12	80.00	110°58'42"	154.96	131.84	N78°23'20"W	N22°53'59"W	S46°07'19"W
15	95	280.00	0°39'10"	3.19	3.19	S63°28'44"E	S63°09'09"E	S63°48'19"E
16	CENTERLINE	300.00	30°35'18"	160.16	158.26	N10°54'02"E	N26°11'41"E	N04°23'37"W
	E. R.O.W.	330.00	30°35'18"	176.18	174.09	N10°54'02"E	N26°11'41"E	N04°23'37"W
	OUTLOT 12	330.00	5°01'59"	28.99	28.98	N23°40'41.5"E	N26°11'41"E	N21°09'42"E
	EXTERIOR	330.00	5°13'27"	30.09	30.08	S18°32'58.5"W	S15°56'15"W	S21°09'42"W
	OUTLOT 11	330.00	20°19'52"	117.10	116.48	N05°46'19"E	N15°56'15"E	N04°23'37"W
	W. R.O.W.	270.00	30°35'18"	144.15	142.44	N10°54'02"E	N26°11'41"E	N04°23'37"W
	92	270.00	19°11'52"	90.47	90.04	N16°35'45"E	N26°11'41"E	N06°59'49"E
	91	270.00	11°23'26"	53.66	53.59	N01°18'06"E	N06°59'49"E	N04°23'37"W
17	OUTLOT 11	1030.00	3°04'14"	55.20	55.20	N78°54'19"E	N80°26'26"E	N77°22'12"E
18	OUTLOT 11	15.00	102°37'50"	26.87	23.42	N51°18'53"W	N00°00'02"E	S77°22'12"W
19	OUTLOT 10	20.00	83°55'48"	29.30	26.75	S41°57'54"W	S83°55'48"W	S00°00'00"E
20	OUTLOT 10	150.00	15°20'59"	40.19	40.07	N88°23'42.5"W	N80°43'13"W	S83°55'48"W
21	OUTLOT 10	234.50	7°51'22"	32.15	32.13	S84°38'54"E	S80°43'13"E	S88°34'35"E
22	CENTERLINE	700.00	12°54'20"	157.67	157.34	N06°27'10"E	N12°54'20"E	N00°00'00"E
	OUTLOT 10	730.00	7°59'05"	101.73	101.65	N03°59'32.5"E	N07°59'05"E	N00°00'00"E
	OUTLOT 11	670.00	8°29'21"	99.27	99.18	N04°14'40.5"E	N08°29'21"E	N00°00'00"E
23	OUTLOT 10	48.00	52°56'28"	44.35	42.79	N36°39'16"E	N63°07'30"E	N10°11'02"E
24	OUTLOT 11	48.00	48°45'02"	40.84	39.62	S13°43'27"E	S10°39'04"W	S38°05'58"E
25	S. R.O.W.	75.00	281°13'28"	368.12	95.18	S77°29'14"E	S63°07'30"W	N38°05'58"W
	OUTLOT 11	75.00	130°34'37"	170.93	136.26	S02°09'48.5"E	S63°07'30"W	S67°27'07"E
	102	75.00	41°47'24"	54.70	53.50	S88°20'49"E	S67°27'07"E	N70°45'29"E
	103	75.00	41°47'24"	54.70	53.50	N49°51'47"E	N70°45'29"E	N28°58'05"E
	104	75.00	41°47'24"	54.70	53.50	N08°04'23"E	N28°58'05"E	N12°49'19"W
	OUTLOT 10	75.00	25°16'39"	33.09	32.82	N25°27'38.5"W	N12°49'19"W	N38°05'58"W



4100 N. Calhoun Rd.  
Suite 300  
Brookfield, WI 53005  
Phone: (262) 790-1480  
Fax: (262) 790-1481

There are no objections to this plat with respect to Secs. 236.15, 236.16, 236.20 and 236.21(1) and (2), Wis Stats. as provided by s. 236.12, Wis. Stats.

Certified \_\_\_\_\_, 20 \_\_\_\_



Department of Administration

CERTIFICATE OF COUNTY TREASURER:

STATE OF WISCONSIN )  
COUNTY OF WAUKESHA ) SS

I, Pamela F. Reeves, being duly elected, qualified and acting Treasurer of the County of Waukesha, do hereby certify that the records in my office show no unredeemed tax sales and no unpaid taxes or special assessments as of this \_\_\_\_\_ Day of \_\_\_\_\_, 20 \_\_\_\_ on any of the land included in the Plat of "SUSSEX PRESERVE ADDITION NO. 4".

Dated this \_\_\_\_\_ Day of \_\_\_\_\_, 20 \_\_\_\_.

Pamela F. Reeves, County Treasurer

CORPORATE OWNER'S CERTIFICATE OF DEDICATION:

MAPLE GROVE PRESERVE, LLC, a Wisconsin Limited Liability Company duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, certifies that said Limited Liability Company has caused the land described on this plat to be surveyed, divided, mapped and dedicated as represented on this plat. I also certify that this plat is required by S.236.10 or S.236.12 to be submitted to the following for approval or objection.

APPROVING AGENCIES:

1. Village of Sussex

AGENCIES WHO MAY OBJECT:

**B-4 CENTRAL MIXED USE DISTRICT**

The B-4 Business District is intended to provide for the orderly and appropriate development of the Central Mixed Use District as designated by the Community Development Authority, in conformance with and to implement "The Downtown Design and Development Plan."

- A. It is the specific intent of the B-4 District to:
1. Encourage development of retail, residential, and office uses that are compatible with the historic Village setting in size, scale, and architectural character.
  2. Encourage the retention and restoration of existing buildings, along with the landscaping and design elements that are characteristic of the Village setting.
  3. Provide opportunities for a diversity of commercial uses and a mix of compatible residential uses.
  4. Encourage mixed-use development on larger tracts in order to fully utilize the area of these tracts for the economic benefit of both the Village and the property owner.
  5. Encourage those types of commercial and office uses that do not generate a traffic volume that may lead to congestion.
  6. Encourage consolidation of driveways, parking, and curb cuts to enhance safety and provide more efficient and economical access and parking.
  7. Encourage the creation of connection driveways parallel to Main Street, to the rear of existing and new development, where appropriate, in order to reduce conflicts between downtown commercial traffic and through traffic.
  8. Minimize visual and functional conflicts between residential and non-residential uses within and abutting the district.
- B. Lot Area and Width  
Lots shall contain sufficient area and width to implement the "Downtown Design and Development Plan," as determined by the Plan Commission and the Architectural Review Board.
- C. Permitted Uses in the B-4 District  
On any lot in the B-4 district, one or a combination of the following uses is permitted:
1. Accommodations and Food Service
    - (a) Hotels and motels
    - (b) Bed and breakfast establishments
    - (c) Restaurants, snack stands, and mobile food services. For a drive-through the Plan Commission must find that the vehicle stacking and noise from its operation will not impact surrounding properties or any public roadway.
    - (d) Bars
    - (e) Food service contractors and caterers
  2. Arts, Entertainment, and Recreation Services
    - (a) Promoter, agent, and artists offices/studio
    - (b) Theater companies and dinner theaters, dance, musical groups, and performing arts companies.
    - (c) Sports teams, clubs and commercial recreational facilities.
    - (d) Museums, historical sites, zoos, botanical gardens, and marinas
  3. Educational, Health Services, and Social Services
    - (a) Fine arts and language schools and studios
    - (b) Sports and recreation instruction



- (c) Automobile driving school
  - (d) General medical services
  - (e) Vocational rehabilitation services
  - (f) Community food services
  - (g) Religious facilities
  - (h) Business, secretarial, computer, training exam, cosmetology, barber and prep schools.
  - (i) Youth, elderly and disability social services and emergency relief services (non-housing) in the B-4 district.
  - (j) Commercial day care centers provided that any outside play area is surrounded by a security fence; that no day care center is located within 300 feet of a gasoline service station, underground gasoline storage tanks, or any other storage of explosive material; that no day care center shall be located in an area where air pollution caused by smoke, dust, gases, or other particulate matter would endanger children; that no day care center shall be located in an area where noise would be so loud, shrill, or have an impulse to endanger children; that traffic be managed in a manner to minimize danger to children; and provided that adequate parking and circulation be provided on the day care facility site in accordance with the standards set forth in Section 17.0603(K)(6)(h)(3) of this Ordinance.
4. Finance, Insurance, Real Estate, and Leasing
- (a) Financial service institutions, for a drive-through the Plan Commission must find that the vehicle stacking and noise from its operation will not impact surrounding properties.
  - (b) Financial investment, insurance offices, and similar financial products
  - (d) Real estate, appraisers, developer offices, and offices of lessors for residential and non-residential properties, excluding lessors of mini-warehouses/self-storage
  - (e) Office equipment rental and leasing
5. General Services
- (a) Repair and Maintenance of consumer electronics, home and garden equipment, appliance, furniture/reupholsters, footwear and leather goods.
  - (b) Barber, beauty, nail salons, spa treatment services
  - (c) Personal care and weight loss services
  - (d) Funeral home and funeral services
  - (e) Coin operated laundries and drycleaners
  - (f) Dry cleaning and laundry services (non-industrial)
  - (g) Photo finishing laboratories
  - (h) General business offices
  - (i) Travel and visitor services
  - (j) General Construction trade services (carpenters, electricians, flooring services, lawn and landscaping services, lighting services, masonry services, painting services, plastering services, plumbing and heating contractors, roofing services, sheet metal services, welding services, and building showrooms)
6. Information Services
- (a) Radio/TV/Cable network, stations, news syndicates excluding towers and dishes
  - (b) Telecommunications services
  - (c) Motion picture and video production
  - (d) Motion picture theaters excluding drive-ins
  - (e) Libraries and archives



- (f) Newspaper, printers, paper and software publishers, recording studio record production, telecommunications services and data processing.
- 7. Professional, Technical, Scientific, and Administrative Services
  - (a) Legal, notaries, and title services
  - (b) Accountants, tax preparation, payroll, and other accounting services
  - (c) Architects, landscape architects, engineering, surveying services
  - (d) Interior, industrial, graphic, and fashion design services
  - (e) Consulting/professional services, advertising, management, HR, marketing, IT
  - (f) Research and development facilities (non industrial)
  - (g) Translation and interpretation services
  - (h) Employment placement and provider services
  - (i) Private investigators, locksmiths, security, and armored car services
  - (j) Janitorial services
  - (g) Pest control services
  - (h) Veterinary services
  - (i) Offices of holding companies and regional managing offices
- 8. Public Administration and Government Services
  - (a) Public utility, offices of Federal, State, and Local Governments
  - (b) Governmental and cultural uses such as fire and police stations, community centers, public works garages, government administration buildings, parks and playgrounds.
- 9. Retail Trade
  - (a) Furniture, flooring, and home furnishing stores
  - (b) Appliances, electronics, camera, office supply and copying stores
  - (c) Home improvement and hardware stores
  - (d) Grocery, supermarkets, convenience, and specialty food stores/markets
  - (e) Liquor/packaged beverage and tobacco stores
  - (f) Pharmacy, drug, beauty supplies, food supplement, and medical supply stores
  - (g) Clothing, shoes, jewelry, luggage/leather goods, formal wear/costume stores
  - (h) Entertainment stores such as books, music, sporting goods, hobby, and video tape/disc/game rental.
  - (i) Gift shops, florists, variety stores, antiques, used merchandise
  - (j) Pet and pet supply stores
  - (k) Art dealers/store
  - (l) Internet sales shopping/mail order business and vending machine sales
- 10. Manufacturing/Assembly
  - (a) Processing and Assembling of Final Products provided that the limited industrial process does not exceed 2,000 square feet and the processing and assembling of final products shall be conducted entirely within an enclosed structure, and there shall be no outside storage of product or materials.
- 11. Transportation and Warehousing
  - (a) Courier, delivery, postal service businesses and U.S. postal Service
- 12. Housing and Mixed Uses
  - (a) Business or professional home offices such as residences of clergymen, architects, landscape architects, professional engineers, real estate agents, artists, teachers, authors,

- musicians, or persons in other recognized professions used to conduct their professions where the office does not exceed 25 percent of the area of only one (1) floor of the residence and only one (1) nonresident person is employed.
- (b) Single-family residential detached home and two-family residential dwelling, but only if the use is constructed prior to January 1, 2010. In the event of a disaster these dwellings may be reconstructed.
  - (c) Conversions. A conversion of an existing single family residential detached building into a combination of non-residential and residential uses or all non-residential uses in a single existing building meeting the following requirements:
    - (1) The front facade and front porch of the existing building, if any, shall be preserved and any building addition shall be located to the rear, and shall be compatible with the existing building in size, scale, and building materials unless the Plan Commission with review and recommendation by the architectural review board determines that the particular façade or front porch is not architecturally significant or if the new proposed façade, front porch, or building addition more appropriately meets the design standards of the Village than the existing structures. Stairways, fire escapes, and other structural alterations shall be located to the rear or side of the building.
    - (2) A single-family detached dwelling may be converted to:
      - (a) A maximum of three non-residential uses with no residential uses, or
      - (b) One apartment with up to two non-residential uses.
    - (3) Residential units must meet the minimum size and facility requirements found in Section 17.0506(A)(15)(g).
    - (4) To encourage a business environment that is compatible with the residential character of the Village, conversions to permitted uses in the B-4 Central Mixed Use District shall not be issued without review and approval of the Plan Commission and Architectural Review Board. Said review and approval shall be concerned with general layout, building plans, architectural and façade treatment, ingress and egress, design, parking, loading and unloading, and sign design and landscaping. The Plan Commission and Architectural Review Board shall determine that all proposed development and redevelopment in the B-4 district will serve to implement the "Downtown Design and Development Plan."
  - (d) A permitted or conditional non-residential use is allowed in accessory buildings located to the rear or side of principal buildings, provided that adequate parking is provided and the non-residential use is owned or operated by a person or entity legally occupying a portion of the principal building.
  - (e) Mixed Use Development. Mixed-use development if it meets the following conditions:
    - (1) The project shall be developed under a single development plan with a unified architectural scheme and site plan, to include building facades, street furniture, signs, lighting standards, parking, driveways

and pedestrian circulation, in conformance with the following requirements:

- (a) Residential and non-residential uses may be combined on a single parcel in new buildings or a combination of new and existing non-residential building(s).
  - (b) Residential uses on the ground floor of buildings in a mixed use development are allowed if the Plan Commission finds that the overall mixed-use development has sufficient retail/office presence to meet the intent of the Downtown Development and Design Plan.
  - (c) When the mixed use involves a conversion of an existing non-residential building the development must establish and or maintain non-residential uses on the ground level of the building(s) such that the Plan Commission finds that the overall mixed-use development will maintain sufficient non-residential presence to meet the intent of the Downtown Development and Design Plan.
  - (d) On large buildings, variations in facades, canopies, and roof lines to provide contrasts of height, color, texture, and materials are encouraged.
  - (e) Any building facade which is visible from a street or other public place shall be constructed of traditional building materials such as local stone, brick, and wood. The use of these materials on other facades is also encouraged.
  - (2) Pedestrian circulation shall be included in the design of the development, with walkways planned for linkage to an existing or future pedestrian network of sidewalks. New sidewalks shall connect to the Bugline Recreation Trail where appropriate and in accordance with the "Downtown Design and Development Plan."
  - (3) A traffic impact study may be required for any development that, in the opinion of the Architectural Review Board or Plan Commission, may generate a traffic volume that may require special mitigation measures.
  - (4) Residential units in mixed use developments shall at a minimum each have one bedroom, one kitchen, and one bathroom. A one bedroom unit shall be at least 600 square feet in size and a two bedroom or larger unit shall be at least 800 square feet in size.
  - (f) Single Family Residential Attached Units, if approved as part of a TIF project plan of the Village.
13. Parking Lots
- (a) Parking Lots without a principal use on the lot are permitted as long as the parking lot supports an adjacent parcels use and is sufficiently screened so as to minimize any impact to any adjacent residential uses.
  - (b) To facilitate traffic circulation:
    - (1) Parking areas for a proposed nonresidential use should be directly connected to nonresidential parking areas on

adjacent lots where possible. If a connection cannot be made when the proposed use is constructed, a driveway may be constructed extending to the adjacent property line in a location where a future connection can be made. Cross-easements for access shall be provided where applicable.

- (2) Driveway access from Main Street or Waukesha Avenue should be limited and shared driveways between abutting lots are encouraged. If only one lot is being developed, a cross-easement to an abutting lot or lots may be offered, and recorded on a plan for the first lot. When such shared driveways are established, no additional driveways shall be permitted to access either lot from Main Street or Waukesha Avenue. Additional driveways may be permitted off a side street or a service street.

- (c) Shared parking facilities maximize the use of the limited area available for parking within the Central Mixed Use District and should be used wherever possible. When parking facilities are shared, the overall parking requirement may be reduced. The number of residential parking spaces, however, shall not be reduced. It shall be the burden of the applicant to provide documentation that uses sharing parking are complimentary and that the full required number of parking spaces would not be required

D. Permitted Accessory Uses

- 1. Off-street parking and loading. Multi-level parking garages shall be designed to minimize impact to adjacent properties and be designed to architecturally match the principal structure.
- 2. Detached garages for storage of vehicles used in conjunction with the operation of the principal business or for occupants of the premises.
- 3. Roof-mounted, solar collectors provided that a registered engineer shall certify that the structure is adequate to support the load.

E. Conditional Uses

- 1. Conditional uses as allowed in Section 17.0500 Conditional Uses.
- 2. No Adult Oriented Establishment except as permitted in accordance with Conditional Uses Section 17.0508.

F. Dimensional Standards.

The following standards shall apply to all uses:

- 1. Build-to Line: For all construction a build-to line rather than a minimum street yard shall be used. New buildings shall be constructed to a line formed by the average building setback of the buildings on the adjacent lots on both sides. If no existing buildings are present on the adjacent lots, the average setback of the next nearest buildings on each side shall be used. The build-to line may be adjusted by the Plan Commission if it finds that a change in the build-to line would be consistent with the Downtown Design and Development Plan, and the Design Standards. The build-to line shall not be less than five (5) feet.
- 2. Side yard: 10 feet on each side, except one or both sideyards may be reduced if the Plan Commission finds that:
  - (a) The Site is masterplanned and provides an efficient use of land,
  - (b) The health, welfare, and safety of the public is not jeopardized by the setback reduction,

- (c) The setback change will encourage pedestrian interaction between buildings.
  - (d) The reduced setback serves to implement the Design Standards and the Downtown Design and Development Plan of the Village.
- 3. Rear yard: 15 feet.
- 4. Loading docks may be required to be set back a greater distance from the side or rear lot line, as determined by the Plan Commission.
- 5. Setback from the Bugline Recreation Trail right-of-way: 15 feet.
- 6. Accessory buildings shall be set back from all property lines at least five (5) feet, except that accessory buildings used for principal uses shall meet all the setback requirements for principal buildings.
- 7. Maximum height: 45 feet.
- 8. Minimum shoreyard: No building or structure shall be located closer than 15 feet to an F-1 Floodway District, F-2 Floodplain Conservancy District, or LCO Lowland Conservancy Overlay District. Where shoreland regulations apply no building or structures shall be located closer than as allowed by Village shoreland regulations.
- 9. Parking lot setbacks:
  - (a) From street right-of-way: The setback of the principal or 25 feet, whichever is less. No parking shall be permitted between the front of the principal building and the street line with the following exception: The Plan Commission may allow up to ten (10) percent of the parking to be in the street yard if it finds that the no street yard parking requirement is unfeasible for the site.
  - (b) From rear property lines: ten (10) feet; except that the Plan Commission may allow a reduction of this requirement to five (5) feet, upon the Architectural Review Board's recommendation of a landscape plan that meets the design standards and intent of the Downtown Design and Development Plan.
  - (c) From side property lines: five (5) feet, however no setback is required if shared parking is used and cross-easements are established.
  - (d) From buildings: five (5) feet.
  - (e) Any parking structure that was constructed prior to the date of the original adoption of the parking lot setback requirements for the B-4 Central Mixed Use District in 1990, that does not meet the current requirements of subsection 17.0419 (F)(9)(a-d), is considered legal non-conforming and does not have to be removed upon performing parking lot restoration work. Extension, expansion, enlargement, reconstruction, substitution, or moving of the parking lot, however, may be subject to said requirements of subsection 17.0419(F)(9)(a-d), as described in Section 17.0900 of this Zoning Ordinance.
- 10. Property line buffer: A buffer area landscaped with trees and shrubs meeting the intent of the "Downtown Design and Development Plan" and the Village Design Standards, shall be required along all side and rear property lines. Driveways may not encroach into the buffer area unless the lot is too narrow to permit any other alternative, in the opinion of the Plan Commission, or for the purpose of shared parking or connected parking areas between lots.

G. Erosion Control

- 1. See Chapter 14 of the Village Municipal Code.

H. Development Design Standards

1. The Village has established clear land use and design principals, as documented in the Village Development Design Guidelines, to guide future development planning decisions towards implementation of the Village's Smart Growth 2040 Comprehensive Plan. These guidelines are intended to serve as basic criteria during reviews, and are not to be construed as the only applicable design elements. All development proposals shall be evaluated against the adopted Village vision of maintaining a small town atmosphere within the Village, featuring a generous amount of greenspace in residential, commercial, and industrial developments.

I. Plans and Specifications to be Submitted to Plan Commission

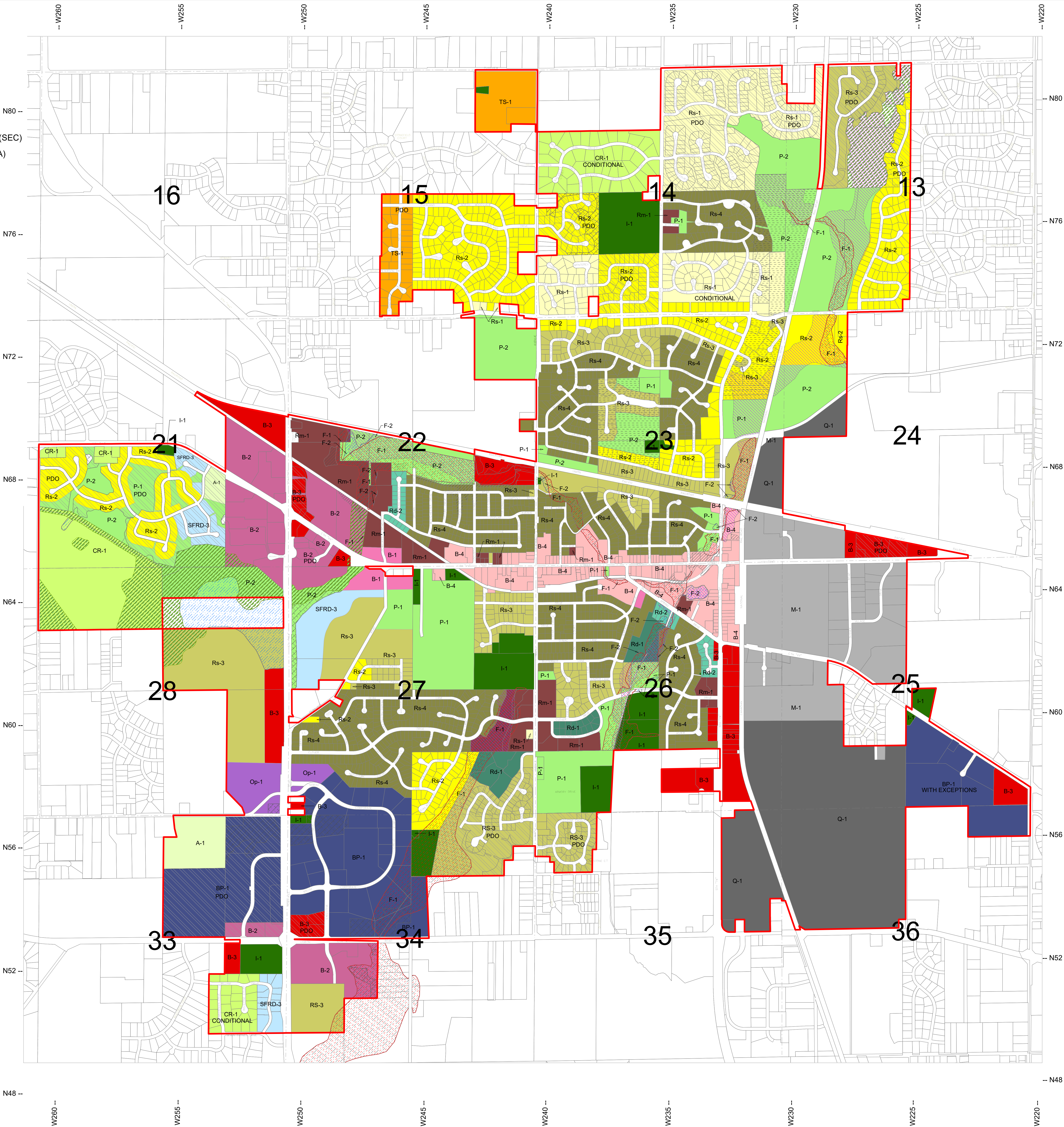
1. To encourage a business environment that is compatible with the residential character of the Village, building permits for permitted uses in the B-4 Central Mixed Use District shall not be issued without review and approval of the Plan Commission. Said review and approval shall be concerned with general layout, building plans, architectural and facade treatment, ingress and egress, parking, loading and unloading, and sign design and landscaping. The Plan Commission and Architectural Review Board shall determine that all proposed development and redevelopment in the B-4 district will serve to implement the "Downtown Design and Development Plan."



Legend

- FLOODWAY (F-1)
- FLOODPLAIN CONSERVANCY (F-2)
- FLOODPLAIN FRINGE OVERLAY (FFO)
- LOWLAND CONSERVANCY OVERLAY (LCO)
- UPLAND CONSERVANCY OVERLAY (UCO)
- PLANNED DEVELOPMENT OVERLAY (PDO)
- SECONDARY ENVIRONMENTAL CORRIDOR (SEC)
- ISOLATED NATURAL RESOURCE AREA (INRA)
- AGRICULTURE (A-1)
- CONSERVANCY RESIDENTIAL (CR-1)
- TRADITIONAL SUBURBAN SINGLE-
- SINGLE-FAMILY RESIDENTIAL (Rs-1)
- SINGLE-FAMILY RESIDENTIAL (Rs-2)
- SINGLE-FAMILY RESIDENTIAL (Rs-3)
- SINGLE-FAMILY RESIDENTIAL (Rs-4)
- SINGLE-FAMILY ATTACHED
- TWO-FAMILY RESIDENTIAL (Rd-1)
- TWO-FAMILY RESIDENTIAL (Rd-2)
- MULTI-FAMILY RESIDENTIAL (Rm-1)
- NEIGHBORHOOD BUSINESS (B-1)
- REGIONAL BUSINESS (B-2)
- HIGHWAY BUSINESS (B-3)
- CENTRAL MIXED USE BUSINESS (B-4)
- BUSINESS PARK DISTRICT (BP-1)
- OFFICE PARK DISTRICT (OP-1)
- INDUSTRIAL (M-1)
- QUARRY (Q-1)
- INSTITUTIONAL (I-1)
- PARK (P-1, P-2)

DATE	ORD.	COMMENT
8/28/1998	540	REZONE M-1 TO B-3
1/4/1999	MISC	YEARLY UPDATE OF ANNEXED LANDS TO A-1
1/26/1999	545	REZONE RS-4 TO RS-2
9/1/2000	543	REZONE A-1 TO RS-1 WITH PDO
9/1/2000	552	REZONE I-1 TO A-1
9/1/2000	554	REZONE M-1 TO B-3
9/1/2000	555	REZONE A-1 TO RS-2 AND RD-1 (Split)
9/1/2000	561	REZONE B-3 TO B-3 WITH PDO
7/25/2000	570	REZONE A-1 TO BP-1
10/5/2000	571	REZONE TO B-3 WITH PDO
11/14/2000	580	COND. ANNEXED LAND TO I-1
7/24/2001	590	COND. REZONE - ADJUST F-1 FP AND FFO DIST.
11/27/2001	599	REZONE A-1 TO RS-1 AND RS-2
2/26/2002	604	REZONE B-3 AND I-1 TO B-4
4/23/2002	605	REZONE A-1 TO CR-1
4/23/2002	607	REZONE B-2a AND P-2
5/28/2002	611	REZONE TO RM-1
7/23/2002	620	REZONE A-1 TO RS-1
10/22/2002	623	REZONE I-1 TO B-4
10/28/2004	633	AMMEND F-1, F-2, AND LCO
3/23/2004	637	REZONE A-1 TO B-5
7/27/2004	654	REZONE B-3 TO B-2a
10/26/2010	736	ANNEXED LANDS TO B-2
2/22/2011	739	REPEAL AND RECREATE ZONING MAP
4/1/2011	MISC	YEARLY UPDATES
4/16/2013	746	ANNEXED LANDS TO B-3
4/16/2013	767	ANNEXED LANDS TO B-2 AND RS-3
7/30/2014	780	AMMEND Q-1, REZONE M-1 TO Q-1
8/19/2014	781	REZONE RM-1 TO B-4
10/8/2015	MISC	Updates
2/24/2015	788	REZONE RS-3 & SFRD-3 TO INCLUDE PDO
2/24/2015	789	ANNEXED LANDS TO B-2
3/24/2015	790	ANNEXED LANDS TO RS-3
4/28/2015	793	REZONE A-1 TO B-3, BP-1, B-2
6/18/2015	802	REZONE RS-2 TO RS-2 WITH PDO
7/28/2015	803	REZONE B-2 TO B-2 WITH PDO
8/25/2015	805	REZONE B-1 TO B-4
12/8/2015	815	REZONE A-1 TO RS-3 WITH PDO
9/27/2016	825	ANNEXED LANDS TO RS-2 & INRA
12/28/2016	828	ANNEXED LANDS TO RS-2 WITH PDO & INRA
1/27/2017	MISC	Basemap updates
6/22/2018	838	REZONE A-1, B-3 TO BP-1 & B-2 W/PDO
8/14/2018	841	REZONE RM-1 TO RS-3 and Basemap Updates
3/12/2019	846	REZONE RS-2 & RS-3 WITH PDO, SEC & LCO
9/24/2019	856	ANNEXED LANDS TO RS-4
4/28/2020	867	ANNEXED LANDS TO RS-4
6/19/2020	864	REZONE VISTA RUN & PUD
5/25/2021	876	ANNEXED LANDS TO A-1 & BP-1 W/PDO

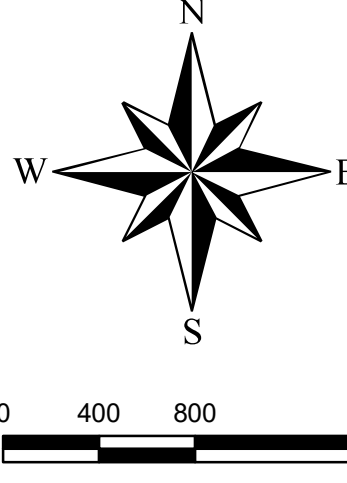


# ZONING MAP FOR THE VILLAGE OF SUSSEX

ATTESTATION:

VILLAGE PRESIDENT \_\_\_\_\_ DATE \_\_\_\_\_

VILLAGE CLERK \_\_\_\_\_ DATE \_\_\_\_\_



Ruekert • Mielke


I, THE UNDERSIGNED, VILLAGE PRESIDENT OF THE VILLAGE OF SUSSEX, WAUKESHA COUNTY, WISCONSIN, DO HEREBY CERTIFY THAT THIS "ZONING MAP, VILLAGE OF SUSSEX, WAUKESHA COUNTY, WISCONSIN", WAS ADOPTED AND APPROVED AS PART OF THE "ZONING ORDINANCE- VILLAGE OF SUSSEX, WISCONSIN", ON FEBRUARY 29, 1998 AND IS AVAILABLE IN THE OFFICE OF THE VILLAGE CLERK. CHANGES THEREAFTER, SHALL NOT BECOME EFFECTIVE UNTIL ENTERED AND ATTESTED ON THIS CERTIFIED COPY.



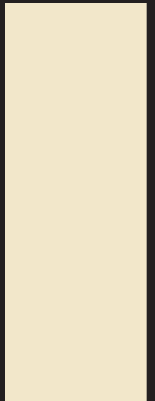
# Sussex

Downtown Design and Development Plan

# Update

Village of Sussex, Wisconsin  
Vandewalle & Associates 

Adopted: July 26, 2011







ORDINANCE NO. 745  
AMENDMENT #6 TO THE 2020 COMPREHENSIVE PLAN  
OF THE VILLAGE OF SUSSEX, WISCONSIN

BE IT ORDAINED BY VILLAGE BOARD OF THE VILLAGE OF SUSSEX, WISCONSIN:

SECTION 1. On March 25, 2003 the Village of Board of the Village of Sussex after meeting all the statutory requirements therefore, adopted its comprehensive Plan pursuant to Wisconsin Statute §66.1001(4)(c); and

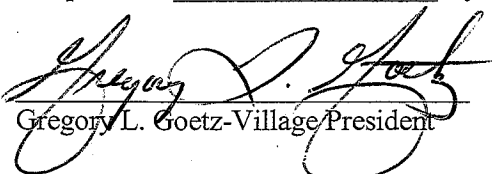
SECTION 2. Wisconsin Statutes §66.1001(4) and 62.23(2) provide that villages may from time to time amend, extend, or add to the comprehensive master plan or carry any part or subject matter into greater detail; and

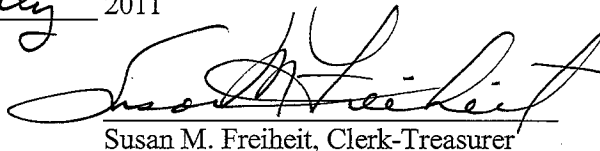
SECTION 3. The Sussex Plan Commission, by a majority vote on June 19, 2011, has adopted a Resolution recommending to the Village Board Amendment #6 to the Village of Sussex Downtown Design and Development Plan which is an addendum to the Comprehensive Plan 2020 in accordance with §66.1001 and 62.23(2) of the Wisconsin Statutes; and

SECTION 4. The Village of Sussex has held a public hearing on July 26, 2011, preceded by Class 1 notice, in compliance with the requirements of §66.1001(4)(d) of Wisconsin Statutes, and the Village of Sussex Community Development Authority held a public open house to receive input from the business community and the public to help update the plan and provided other opportunities for public involvement pursuant to its adopted public participation strategy and procedures; and

SECTION 5. This ordinance shall take effect upon passage by a majority vote of the members-elect of the Village Board and publication as required by law.

Adopted this 26th day of July 2011

  
Gregory L. Goetz-Village President

  
Susan M. Freiheit, Clerk-Treasurer

Published 3rd day of August 2011

Number of ayes 6

Number of nays 0



# ACKNOWLEDGEMENTS

2003 Village Board Trustees:

Ralph Benka  
Vicki Braden  
Fred Gallant  
Greg Goetz  
Roger Johnson  
Al Olmstead

2003 Village President:  
Michael Knappi

2003 Village Administrator:  
Chris Swartz

2003 Community Development Authority:

Dennis Janssen  
Roger Johnson  
Nancy Kolesari  
Cheryl Schroeder  
Joan Majdecki  
Christina White

Prepared by: Vandewalle & Associates

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Justin Yonker  
(Cartography)

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Steve Pellechia  
Robert Schlei  
Richard Wegner  
Christina White  
Roger Johnson  
Mike Knapp

## 2011 IMPLEMENTATION SUPPLEMENT ACKNOWLEDGEMENTS

Sussex Community Development Authority:

Ralph Benka  
Jesse Blount  
Chad Engstrom  
Jim Pino  
Arnold Vaske  
Tony Lapcinski, Village President (2007-2011)  
Pat Tetzlaff

Planning Assistance by:

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Kristina Surfus, Assistant Planner

Village Staff:

Jeremy Smith, Village Administrator  
Tony Lapcinski, Village President (2007-2011)  
Greg Goetz, Village President (2011- )

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# I N T R O D U C T I O N

The Sussex Downtown Design and Development Plan Update (2003 and May 2011 update) is an update to the Sussex Downtown Design and Development Plan of 1996. The Village should also consider the Sussex Design and Development Plan Update as an addendum to the Village of Sussex Comprehensive Plan 2020 (Adopted 2003).

## UPDATE

This document is intended to assist public officials who are guiding the future vision of Sussex, planning the Village's public improvements, and reviewing design proposals for approval. It is also intended to guide developers as to the Village's expectations for development type, location, design character, and quality.

The Sussex Downtown Design and Development Plan Update describes a vision for the future role and identity of downtown Sussex, and provides a conceptual framework for planning the downtown's future. The Plan describes design strategies, layouts, and guidelines for specific redevelopment areas and sites; identifies public improvement projects; and recommends steps for implementation.



## ASSESSMENT

Like many former farming communities on the Metropolitan fringe, Downtown Sussex is at a pivotal point in its history. The westerly growth of Waukesha County puts the Village squarely in the path of new development. Some Village residents may bemoan the forces of growth; others see an opportunity to showcase local businesses, improve services to area residents, and infuse new life into the downtown. As growth continues, downtown Sussex is clearly on the verge of discovery by new residents seeking the Village's inherent qualities: a small town ambiance, urban conveniences, a visible history, and a strong sense of semi-rural sophistication.

It is important that the Village shape new development in a way that respects local history and the sensibilities of local residents. Nowhere is this more important than in the downtown area, symbol of the civic life of the community.







D O W N T O W N   T O D A Y

# IDENTITY: GROWN IN A HISTORIC SETTLEMENT

Located at the western edge of the Milwaukee Metropolitan area, and a fair distance north of I-94, the Village has strong regional name recognition, but an indistinct visual image. Indeed, in a county and state known for Indian names, Sussex is perhaps most easily distinguished by its name rather than by any familiar events or landmarks. The name 'Sussex' has a decidedly Norman-Anglo ring to it, while its form of government, 'Village,' has English overtones. The heavy stonework and wrought iron used in various places in the Village reinforces this theme. This iconography could suggest special themes for programmed events in the downtown.

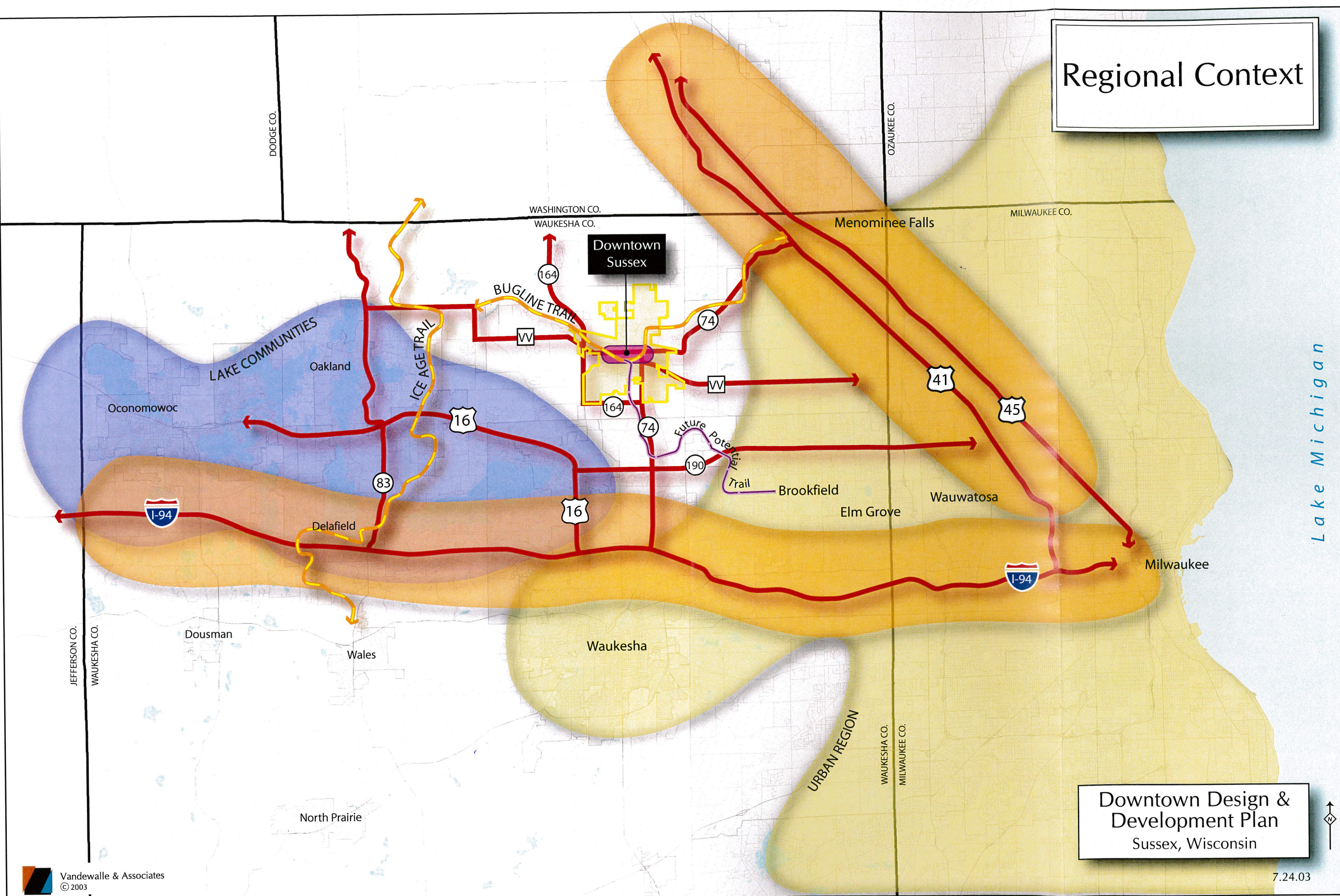
Currently, downtown Sussex is not considered a regional center on the order of Hartland or Wauwatosa. As westward expansion of Waukesha County continues, however, the Village represents one of just a few places in the area where growth can coalesce around a real historic settlement.

From a local perspective, downtown Sussex serves as a district for basic services, convenience shopping, and local government. Anecdotal information suggests that most patrons are local residents who live in, or directly adjacent to, downtown; whereas a handful of establishments, such as Piggly Wiggly, auto repair shops, may draw customers from a wider area.





# Regional Context



Downtown Design & Development Plan  
Sussex, Wisconsin





Downtown Sussex is somewhat unique for the area, in that it represents a walkable environment where multiple errands can be attended to in a single trip. It is also an area where encounters with neighbors and other acquaintances are more a matter of routine than of chance. The Village Hall, the Library, the Grocery store, and the many shops

## ROLE: A WALKABLE VILLAGE CENTER

along Main Street create a complementary mix of activities that is distinctly traditional and Village-centered. These are exactly the types of characteristics that are being designed (with mixed success) into numerous new "neo-traditional" developments throughout the country.

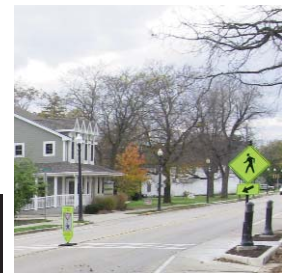
The public input received during the development of this plan suggests that these are the ideals that the Village ought to promote as it works to vitalize its downtown area.

Adaptive reuse of older buildings, traffic calming techniques, traditional neighborhood design, and economic diversification/expansion

are some of the key issues on the minds of local residents. There is also a sense that considerable amounts of local discretionary income is leaking out of the community due to a mismatch between the purchasing power of local households and the lack of purchasing opportunities in the downtown.



i d e a l s



# TODAY: A HISTORIC CROSS-SECTION

**D**owntown Sussex today is neither urban nor rural. Essentially, it is a loose collection of historic structures grouped in individual clusters along Main Street. These groups, or sub-districts, are the major focus areas of this plan. They include: the intersections of Waukesha Avenue and Main Street; Silver Spring Drive and Main Street; and Maple Avenue and Main Street.

The project area boundary is centered on Main Street between Pewaukee Road on the west, and the Wisconsin Central Railroad on the east. The area includes adjacent entry corridors, parks and open space, and neighborhoods.

Because Main Street intersections are widely spaced, the activity pattern in downtown is dispersed. There is also no discernable middle-point to downtown. The downtown area does include an interesting assortment of historic architecture, including Queen Anne, Italianate, Greek Revival, Craftsman, and various Midwestern vernacular styles, along with some interesting examples of early roadside architecture. Many historic residences along Main Street have been converted to light commercial uses, such as small shops and professional offices.

While downtown lacks a true center, it does have several features that give it a strong urban design framework on which to build.

Besides the historic architecture, these include: points of entry that are well defined (demarcated by changes in road elevation and groups of distinctive buildings that bracket either end of Main Street); a three-point intersection at roughly the middle point of Main Street that could provide a focal point for (and axial views of) important buildings; and the Bugline Trail which straddles the southern edge of downtown and links the community to points east and west.

The wide use of Lannon stone and wrought iron also gives the Village a strong artisan flavor. New development in downtown should seek to further define and accent these features.



# D O W N T O W N   T O M O R R O W



# A COLLECTIVE VISION

## RELATIONSHIP TO OTHER PLANS

This plan builds upon several previous local planning efforts including most recently, the Village of Sussex Comprehensive Plan 2020. That plan prescribes a number of goals and policies related to the downtown which are expanded upon, and physically detailed in this plan. The Comprehensive Plan also identifies the downtown as a "Smart Growth" area where incremental redevelopment is likely to occur. The Downtown Design and Development Plan therefore can be thought of as a detailed district plan for an area singled out for special attention by the Comprehensive Plan.

Comprehensive plans are by nature general in scope, and provide the policy framework for more specific district plans such as this. Although no single plan takes precedence, it is recommended that this plan be adopted as an addendum to the comprehensive plan to avoid any confusion over the relationship between the two, and to afford this plan the same legal powers extended under the State's "Smart Growth" statute.

## DOWNTOWN DESIGN PLAN

The design plan for downtown contemplates a more active, functional, and visually unified Village center. Downtown Sussex should be a place that is instantly recognized as the community's major activity area and public living room. It should be a place of centrality that accommodates a variety of uses and activities throughout the day, and causes passers-by to pause and take notice of the buildings, people, and activities located there. In both appearance and function, it should present a strong contrast to single-use suburban shopping areas, and be scaled to the needs and movement patterns of pedestrians. Downtown patrons should include both local residents conducting everyday business, and day tourists seeking an interesting destination in which to pass a weekend afternoon or evening.

The downtown district should be short in length, and offer a concentrated number of places and activities that encourage walking and impulsive purchasing. It should have clearly marked entrances and exits, and be set off



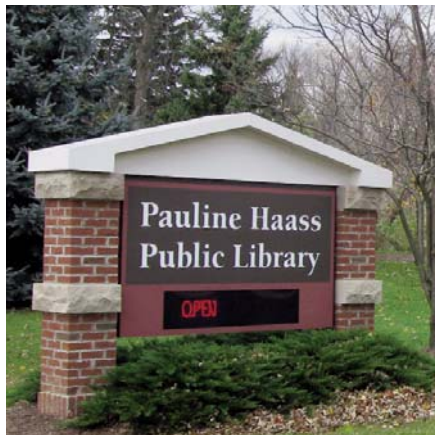
# A COLLECTIVE VISION

from the rest of Main Street through the form, scale, and placement of its buildings; the density of shops and activities; and different pavement treatments and textures.

Although the principal domain of pedestrians and bicyclists, downtown should also be convenient and accessible via automobile. Downtown must invite the casual user as well as the convenience-seeker wishing to run multiple errands in a single trip. The physical arrangement of buildings and other built features must be livable and friendly, as well as offer places of refuge and privacy. Downtown needs to be safe, comfortable, and explorable.

Public input received during the course of this plan revealed the following needs for the downtown area:

- Provide more public gathering places and events
- Create more local shopping
- Protect, preserve, and celebrate historic downtown buildings
- Offer more entertainment venues and specialty businesses
- Initiate transportation management and traffic calming
- Create design improvements to public space (streetscaping and Civic Center)
- Offer programmed downtown activities
- Preserve historic small town atmosphere
- Expand the mix of land uses and things to do
- Incorporate/extend the Bugline Trail
- Promote more walking and biking to/from the downtown



# DOWNTOWN CONCEPT PLAN

Although the purpose of the Plan is to provide a harmonious planning framework for all of Downtown, it seeks to build upon some unique areas already found there. Described below are several of these unique areas, each with its own character. The districts, which include “core” and “transition” districts, are organized along Main Street, Silver Spring Drive, Waukesha Avenue, and the Bugline Trail. Together, they form a grouping of activity centers, each with its own identity and role, which comprise downtown Sussex.

## COMMERCIAL DISTRICTS

### Four Corners Traditional Commercial District

The Four Corners Traditional Commercial District is the western anchor of downtown. Located between the western gateway to downtown and the Civic Core, and focused at the intersection of Maple and Main, this area is the historic center of Sussex. The district contains historic assets and a variety of reuse and redevelopment opportunities. (See Downtown Design and Development for more specific recommendations.)

- Main Street Redesign (Traffic Calming)
- Historic Crossroads
- Commercial Reuse Guidelines (Historic Residential Structures)
- Short- and Long-Term Redevelopment at three quadrants of the intersection
- Community Image
- Cemetery Preservation Restoration
- Historic Walking Tour

### The “Point” (Silver Spring Corners) District

The Point District is located in the heart of downtown Sussex. Located between the Civic Core and the East Main Hilltop District, the area is centered at the crossroads of Main Street and Silver Spring Avenue. The district contains community facilities, Bugline Trail connections, Sussex Creek, and a variety of redevelopment opportunities. (See Downtown Design and Development for more specific recommendations.)

- Village Central Intersection
- Civic Identity
- Commercial Opportunities
- Sussex Creek
- Short- and Long-Term Redevelopment
- Integration of Bugline Trail

### Cannery Crossing District

The Cannery Crossing District is the eastern anchor of downtown Sussex. Located at the crossroads of Main Street and Waukesha Avenue and the eastern gateway to downtown, the area was historically the depot and entertainment center of the community. The district contains employment centers, a state highway, and a variety of redevelopment opportunities. (See 2011 Implementation Supplement for more detailed and updated concepts for this area.)

# DOWNTOWN CONCEPT PLAN

## New Commercial:

- Traditional Restaurants
- Mammoth Springs Residential
- Connection to Quad Graphics
- Rail Connection
- Bugline Trail Intersection
- New Intersection Design
- Architectural Standards

## CORE AND TRANSITION DISTRICTS

### Sussex Center District

The Sussex Center is the geographical and symbolic center of downtown Sussex. The Village Hall, the Library, and the Sussex Civic Center Park create a civic campus where the community business is done and community events are held. (See 2011 Implementation Supplement for more detailed and updated concepts for this area.)

- Enhance "civic presence"
- Maintain open space of park for community events
- Enhance access to creek at Sussex Civic Center Park
- Enhance Village Hall appearance on Main Street
- Coordinate signage throughout civic campus
- Define pedestrian pathways throughout civic campus

See details on page 46, Sussex Civic Center Park.



### West Main Hilltop District

The West Main Hilltop District is primarily a residential area located on a hill that separates the West Main Street Entry Corridor and the downtown proper. It creates an effective west gateway to the center of downtown Sussex.

- Plan for long-term redevelopment of properties on south side of Main Street
- Implement secondary streetscape improvements (as shown)

### East Main Hilltop District

The East Main Hilltop District is primarily a residential area located on a hill that separates The Point District and Templeton Corners Commercial District. It creates an effective eastern gateway to the center of downtown Sussex.

- Maintain historic homes
- Implement secondary streetscape improvements (as shown)





# DOWNTOWN CONCEPT PLAN

## CORRIDORS

### West Main Street Entry Corridor

The section of Main Street (County VV), located between development related to Highway 164 and Locust Street, is an important entry corridor to downtown. It carries a high volume of traffic, and sets the tone and image for the east-bound entry to downtown.

- Establish design guidelines for commercial properties fronting on Main Street
- Implement secondary streetscape improvements (as shown)
- Enhance Fire Station property
- Enhance Village Park frontage
- Provide downtown welcome signage
- Enhance Bugline Trail crosswalk

### Silver Spring Entry Corridor

The section of Silver Spring Drive (County VV), located between Highway 74 and Main Street, is an important entry corridor to downtown. It carries a high volume of traffic and sets the tone and image for the west-bound entry to downtown.

- Establish design guidelines for properties fronting on Silver Spring Drive
- Implement primary streetscape improvements (as shown)
- Provide downtown welcome signage

### Highway 74 Entry Corridor

The section of Waukesha Avenue (Highway 74), located between Silver Spring Drive and Main Street, is an important corridor adjacent to downtown. Future development in the Templeton Corners Commercial District, high traffic volume, and rail access make long-term development possible in this corridor.

- Establish design guidelines for properties fronting on Waukesha Avenue
- Provide wayfinding signage to downtown

### Maple Avenue Entry Corridor

This section of Maple Avenue, located between Main Street and the elementary school, is an important secondary entry corridor to downtown.

- Implement primary streetscape improvements between Main Street and the Bugline Trail (as shown)
- Provide downtown wayfinding signage to downtown

### Bugline Trail

The section of the Bugline Trail, which travels through downtown Sussex, is an important recreational corridor. The Village should complete a continuous trail through downtown, and make connections between the trail and adjacent neighborhoods and downtown activity centers.

# DOWNTOWN CONCEPT PLAN

## Sussex Creek Environmental Corridor

The Sussex Creek corridor is an important downtown amenity. The Village should maintain the quality of the environmental corridor and utilize the creek for recreational uses and a visual resource.



## Sussex Village Park

The large Sussex Village Park is an important downtown amenity and activity area. The Village should utilize park frontage to improve the image of the downtown, and make connections between the park and the downtown's public, commercial, and residential properties.

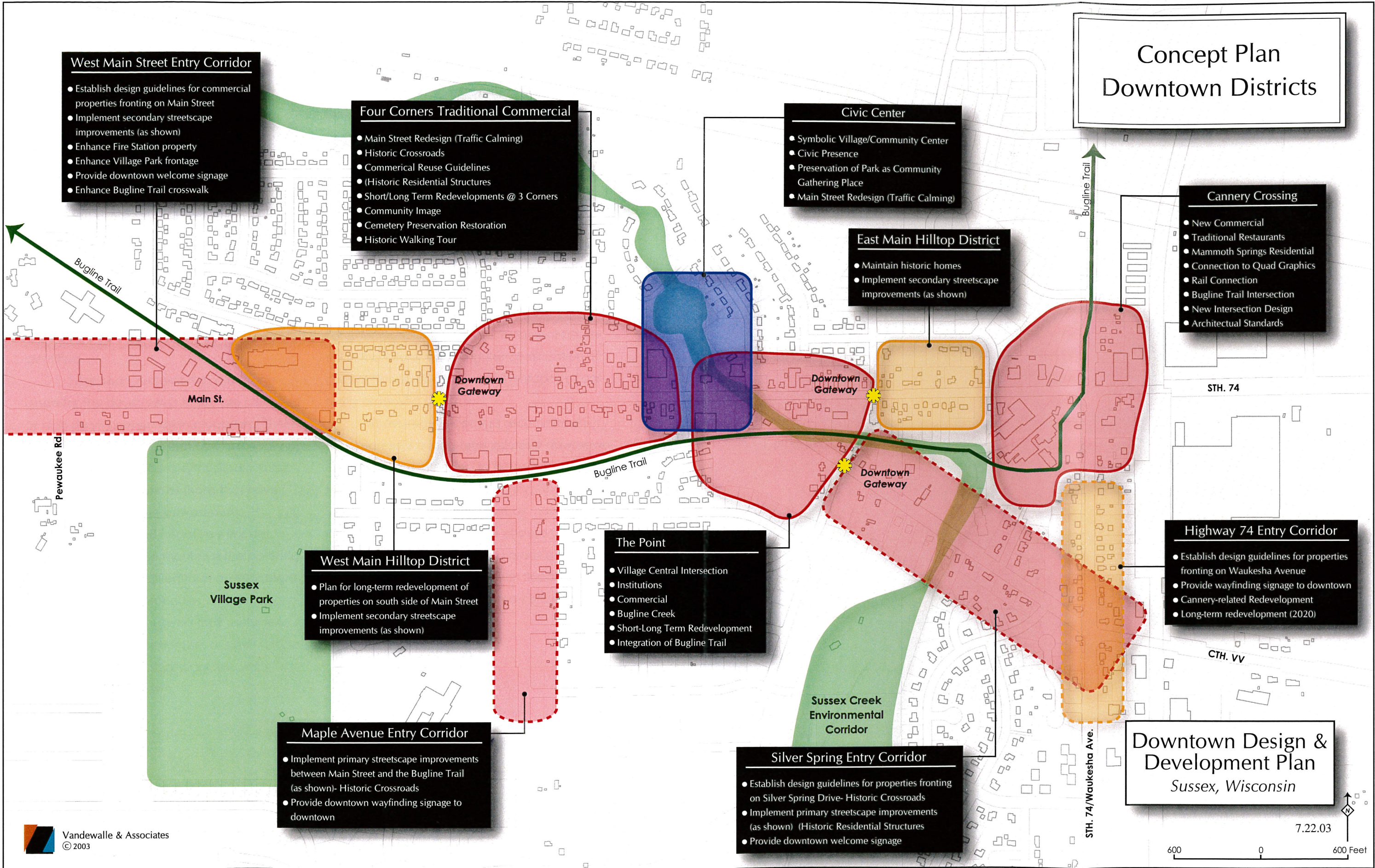


## Surrounding residential neighborhoods

The close proximity of surrounding residential neighborhoods to the downtown offers an opportunity to connect the two and increase downtown business and social activity. Improving connections between the hundreds of homes within several blocks of the downtown will increase business traffic, and will help make downtown a social center for the community.







**West Main Street Entry Corridor**

- Establish design guidelines for commercial properties fronting on Main Street
- Implement secondary streetscape improvements (as shown)
- Enhance Fire Station property
- Enhance Village Park frontage
- Provide downtown welcome signage
- Enhance Bugline Trail crosswalk

**Four Corners Traditional Commercial**

- Main Street Redesign (Traffic Calming)
- Historic Crossroads
- Commercial Reuse Guidelines
- (Historic Residential Structures)
- Short/Long Term Redevelopments @ 3 Corners
- Community Image
- Cemetery Preservation Restoration
- Historic Walking Tour

**Civic Center**

- Symbolic Village/Community Center
- Civic Presence
- Preservation of Park as Community Gathering Place
- Main Street Redesign (Traffic Calming)

**East Main Hilltop District**

- Maintain historic homes
- Implement secondary streetscape improvements (as shown)

**Cannery Crossing**

- New Commercial
- Traditional Restaurants
- Mammoth Springs Residential
- Connection to Quad Graphics
- Rail Connection
- Bugline Trail Intersection
- New Intersection Design
- Architectural Standards

STH. 74

**Highway 74 Entry Corridor**

- Establish design guidelines for properties fronting on Waukesha Avenue
- Provide wayfinding signage to downtown
- Cannery-related Redevelopment
- Long-term redevelopment (2020)

CTH. VV

**Downtown Design & Development Plan**  
*Sussex, Wisconsin*

**West Main Hilltop District**

- Plan for long-term redevelopment of properties on south side of Main Street
- Implement secondary streetscape improvements (as shown)

**The Point**

- Village Central Intersection
- Institutions
- Commercial
- Bugline Creek
- Short-Long Term Redevelopment
- Integration of Bugline Trail

**Sussex Creek Environmental Corridor**

**Silver Spring Entry Corridor**

- Establish design guidelines for properties fronting on Silver Spring Drive- Historic Crossroads
- Implement primary streetscape improvements (as shown) (Historic Residential Structures)
- Provide downtown welcome signage

**Maple Avenue Entry Corridor**

- Implement primary streetscape improvements between Main Street and the Bugline Trail (as shown)- Historic Crossroads
- Provide downtown wayfinding signage to downtown

**Sussex Village Park**







# DOWNTOWN DESIGN AND DEVELOPMENT

While the Concept Plan provides a framework for downtown Sussex, the following analysis and conceptual designs for three commercial districts provide specific strategies for redevelopment and preservation.

## PRESERVATION AND REDEVELOPMENT ANALYSIS

One purpose of the Master Plan is to help Sussex "manage change" by deciding what to keep . . . and what to change. As a part of this Master Plan process, high priority preservation and redevelopment properties have been identified.

Sites and structures that are strong assets for the downtown's future are identified as high-priority preservation properties. These may include properties that are: historically or architecturally significant, symbolically important to the community, or those that have experienced major new investment.

Properties that negatively impact downtown, and those that have the potential to be assets are identified for redevelopment. These may include properties that are: blighted, have otherwise outlived their usefulness, have a higher and better use, or have a location that can play a significant role in the health of the downtown.

Because redevelopment is a process that occurs over time, these properties are prioritized. Short-term priorities include projects already in the approval process, and those with current developer interest. They include projects that will have an immediate impact both visually and economically, and are likely to spawn further reinvestment. Mid-term priorities are also highly significant to downtown, have immediate development potential, and should be proactively planned by the Village. Long-term priorities have development potential in the future, assuming the continuation of current trends, and the implementation of currently proposed projects.

These redevelopment recommendations represent a long-term vision for downtown Sussex and are intended to guide and coordinate future development planning. This vision will be implemented over time as market forces dictate and property owners choose to redevelop parcels and assembled areas of downtown. (See 2011 Implementation Supplement for more detailed and updated concepts.)



# DEVELOPMENT & DESIGN PRINCIPLES

A number of general design principals should help guide new development in downtown Sussex. These include the following:

- Avoid deep setbacks for new buildings. This will help add physical definition and visual interest to Main Street.
- Consolidate parking on a few shared lots to provide a more compact and cohesive streetscape. Encourage rear-lot parking.
- Vary the footprints, heights, and massing of new buildings to promote a mixed use development pattern.
- Increase the number of access points to the Bugline Trail from Main Street and other points downtown.
- Accent the main entry points to downtown through a more concentrated application of streetscape elements, plantings, and signage.
- Slow traffic through downtown by placing devices and visual cues at key points in the pedestrian circulation system.
- Further promote the use of Lannon stone and wrought iron as the primary design motif for future streetscape improvements.
- Introduce basic wayfinding elements along Highways 74 and VV, so that drivers realize that they are approaching a *destination*, and so they know how to get there.



# DOWNTOWN DESIGN AND DEVELOPMENT

## TOWNSCAPE: GENERAL GUIDELINES

As stated earlier, there are a number of remarkable historic structures in Downtown but no single architectural style predominates. Instead, the Village contains a great deal of architectural diversity and eclecticism.

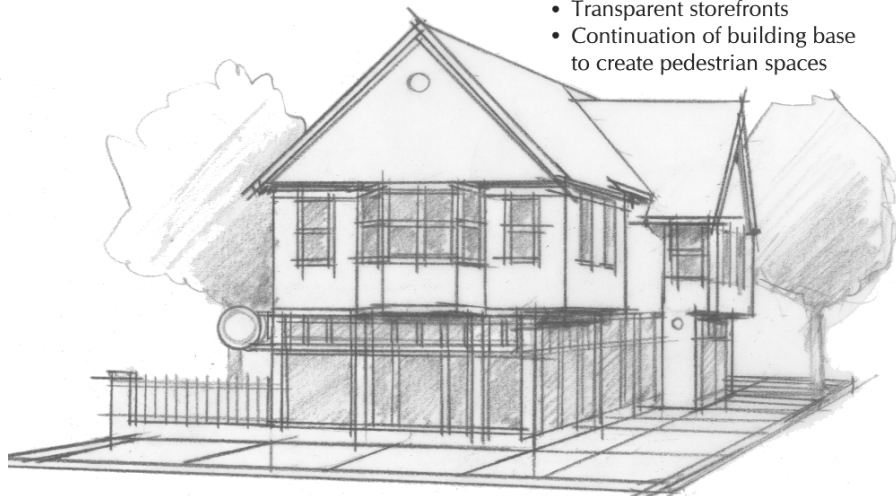
Mandated adherence to a specific architectural style or "theme" when no dominant style exists is an invitation to kitschy historicism, or to put it more bluntly, fake "old timey" architecture. Instead of focusing on architectural pastiche, a more effective way to introduce a consistent visual framework is to continue pronounced land use patterns and create a strong public armature consisting of uniform street and sidewalk surfaces (treatments and dimensions), relatively consistent lot and block patterns (size, spacing, and setbacks of buildings), and consistent streetscaping (e.g. lights, signs, benches, trash receptacles). A strong uniformity in these elements is usually enough to tie together an otherwise disparate collection of buildings. Architecturally, a stronger focus on overall building form, scale, orientation, and materials may make more sense than emulating a specific style that may or may not be "original" to the Village.

Although lacking a dominant style or building typology, the Village does have some surviving agrarian structures and imagery that may provide the rudiments of a basic architectural vocabulary. These elements consist of heavy Lannon stone buildings such as the implement dealership, and the occasional extant silo or outbuilding. These masonry structures along with the frequent use of wrought iron give the Village an artisan feel whose character should carry through to new buildings. The name of the Village may also provide some basic direction as it connotes the understated architecture of English country towns. Generally, these buildings should be simple in overall form and observe a few key design rules:

- Strong gabled roof forms especially for non-commercial buildings. Gable end, or cross-gable facing the street. Overhangs appropriate. (Steep roof pitch + 45 degrees). Dormers preferred on gable sides.

### Sussex townscape & theme:

- Street presence
- Simple overall building form
- Prominent gable roof form
- Projecting bays, bay windows, & overhangs
- Transparent storefronts
- Continuation of building base to create pedestrian spaces



- Frequent use of exterior masonry, preferably rough-hewn stone, but also brick and stucco (particularly on base courses and foundation walls). Cream colored preferred.
- Use of natural materials (wood, glass, stone) instead of synthetic substitutes
- Well articulated doorways and windows (vertically oriented upper story windows, proportional to other structural openings).
- Front facades of commercial buildings containing a minimum of 50% window and door openings (Proportionally less on visible side elevations).
- Definition and articulation of basic structural elements such as rooflines, doorways, lintels, piers, and bays but with a minimum of superficial or non-structural decoration (i.e. "gingerbread").
- Double hung, or casement windows

Again, given the number of styles present in the Village (and since a number of architectural types can fit within the structural envelop described above including Dutch Colonial and a host of other variants), stylistic fussiness should take a back seat to more critical considerations such as building placement, orientation, massing, scale, materials, and roof form. Contemporary interpretations of traditional architectural forms should be favored over false replications of historic buildings.



# DOWNTOWN DESIGN AND DEVELOPMENT

## DOWNTOWN DESIGN DISTRICTS

### Four Corners Traditional Commercial District

This intersection marks the western end of downtown. It is the historic main crossroads of the community and a principal point of entry to the downtown. This area contains a variety of short- mid- and long-term redevelopment opportunities as well as several historic structures that should be preserved and/or incorporated into new development. Many of the historic structures along Main Street may be adapted to uses other than residential.

This area should be viewed as the western doorstep of downtown, and should have a traditional "Main Street" appearance. The design focus should reflect the scale, development pattern, and architectural history of the area. New development should bear a strong relationship to nearby structures though consistent scale, massing, materials, placement, and repetition and/or alignment of prominent structural or ornamental elements. Corner buildings should wrap their respective corners, and pres-

ent a face to both Main Street and Maple Avenue. Through-lot passageways should connect Main Street to the Bugline Trail and adjacent properties wherever possible. Shared parking on rear or side lots should be encouraged as an alternative to individual on-site parking lots.

Where individual site conditions allow, residential infill development should continue to occur between buildings lining the south side of Main Street and the Bugline Trail.

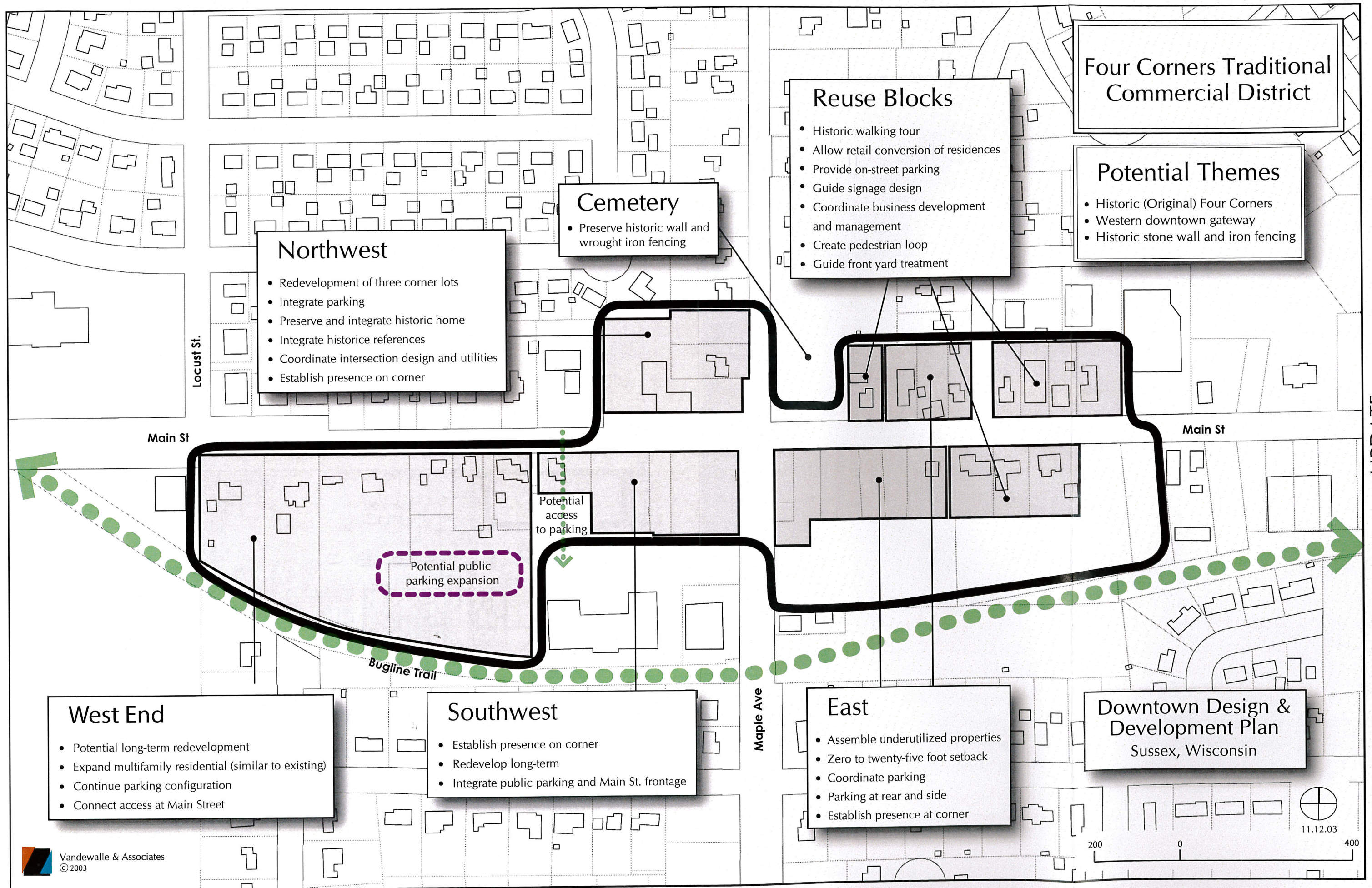
The existing properties on both sides of Main Street, between Maple Avenue and the Public Library, are an important asset to downtown Sussex. Several businesses have been successfully established in this area, with the potential for more. Many of the properties are either currently underutilized or have the potential for reuse. High traffic volume on County VV and new adjacent residential projects create opportunities for coordinated redevelopment and reuse. The following strategies and guidelines should be implemented for successful future redevelopment of this site.











**Northwest**

- Redevelopment of three corner lots
- Integrate parking
- Preserve and integrate historic home
- Integrate historic references
- Coordinate intersection design and utilities
- Establish presence on corner

**Cemetery**

- Preserve historic wall and wrought iron fencing

**Reuse Blocks**

- Historic walking tour
- Allow retail conversion of residences
- Provide on-street parking
- Guide signage design
- Coordinate business development and management
- Create pedestrian loop
- Guide front yard treatment

**Four Corners Traditional Commercial District**

**Potential Themes**

- Historic (Original) Four Corners
- Western downtown gateway
- Historic stone wall and iron fencing

**West End**

- Potential long-term redevelopment
- Expand multifamily residential (similar to existing)
- Continue parking configuration
- Connect access at Main Street

**Southwest**

- Establish presence on corner
- Redevelop long-term
- Integrate public parking and Main St. frontage

**East**

- Assemble underutilized properties
- Zero to twenty-five foot setback
- Coordinate parking
- Parking at rear and side
- Establish presence at corner

**Downtown Design & Development Plan**  
Sussex, Wisconsin





# DOWNTOWN DESIGN AND DEVELOPMENT

## MAIN STREET FOUR CORNERS SITES

### Strategies

- Unified commercial development
- Symbolic core
- Reuse of existing structures
- Revitalized Main Street frontage
- Streetscape as unifying element
- Connection with community pedestrian system

- Parking: At side and rear, on-street
- Service areas: Screened
- Fencing: Height and color palette
- Provide connection with Bugline Trail
- Coordination with public streetscape
- Roof

See specific guidelines for Corner Retail or Typical Commercial Lots, Signage, and Parking in Appendix A.

### Guidelines

- Sensitive reuse of existing structures
- Setback at Main Street: Existing/variable
- Materials: Restoration of existing
- Height: Existing, one-to-two stories
- Signage: Limited size, Site location, integrated with building





# DOWNTOWN DESIGN AND DEVELOPMENT

## THE POINT (SILVER SPRING CORNERS) DISTRICT

This area marks the geographical center point of Downtown. It is generally defined as the area between the Library and a block east of the angled intersection of Main Street and Silver Spring Drive. It also encompasses the Sentry store, several other retail establishments, and the main portal to the Bugline Trail.

New development in this area should seek to further accent this area as the symbolic heart of the Village. Future uses should seek to create a strong interrelationship between the civic institutions, the creek, and the Bugline and nearby retail and residential buildings. This should be an area with the heaviest pedestrian activity in the Village, and nearby uses should be of the type (and should be designed) to take advantage of high visibility and pedestrian traffic. Ample pedestrian appointments, including traffic-calming devices, should be incorporated into this area.

The civic focus of this sub-district should be given further emphasis through the creation of a community focal point or gathering area centered on or near the creek. All new build-

ings however, should be clearly oriented toward Main Street, with a special importance attached to corner buildings. Proposed buildings on sites that are directionally on-line with the end of Silver Spring Drive should also be held to the highest design standards because of their extremely high visibility. Although it should continue to be accessed from the rear, the Village Hall should attempt to present a more welcoming 'face' to Main Street.

The existing Piggly Wiggly site is an important asset to downtown Sussex. Its proximity to Main Street, the Village park, and Sussex Creek, and the Village Hall make it valuable commercial property, and highly influential on the character of downtown Sussex. The following strategies and guidelines should guide redevelopment of this site. (See 2011 Implementation Supplement for more detailed and updated concepts for this area.)

## Piggly Wiggly Site

- Coordinate traffic access with Village Hall/Library entry drive
- Locate new structures on Main Street R.O.W. (zero setback) to dimension matching existing buildings to the west
- Locate parking at rear and/or side
- Provide pedestrian and bicycle connection between site and Bugline Trail
- Explore integration of existing structure for development cost savings
- Manage site's storm water in order to protect creek  
Utilize creek as natural amenity

## Potential Themes

- Village center
- Bugline Trail
- Community institutions

## The Point District

## North

- Assemble sites at northeast corner of Main Street and Orchard Drive
- Explore integration of existing structures for development cost savings
- Redevelop existing corner site with landmark structure
- Locate parking at rear and/or side
- Explore potential pedestrian connection with Hillview Road
- Provide vehicular access from Orchard Drive
- Locate new structures on Main Street R.O.W. (zero setback)
- Consider shared public parking area

## East

- Redevelop "point" site (long-term). Explore potential integration with Community Youth Center site
- Explore assemble of sites to the east of Community Youth Center for long-term commercial use

## South

- Connect Pembroke Street through to Hickory Drive. Coordinate traffic circulation with Post Office
- Provide pedestrian and bicycle connection between south neighborhood and Bugline Trail. Align with existing pedestrian connection to Main Street
- Continue pattern of single family lots on south side of Pembroke Street and on north side of Pembroke Street west of pedestrian connection to the Bugline Trail
- Develop mixed-use residential and neighborhood commercial north of Pembroke Street and east of pedestrian connection to the Bugline Trail
- Continue downtown streetscape improvements along Silver Spring Drive between Main Street and Hickory Drive (Public)
- Consider shared public parking area

## Central

- Redevelop residential site with landmark structure
- Integrate with existing office and parking
- Integrate with existing small park and Bugline Trail

## Downtown Design & Development Plan Sussex, Wisconsin





# DOWNTOWN DESIGN AND DEVELOPMENT

## PIGGLY WIGGLY SITE

### Strategies

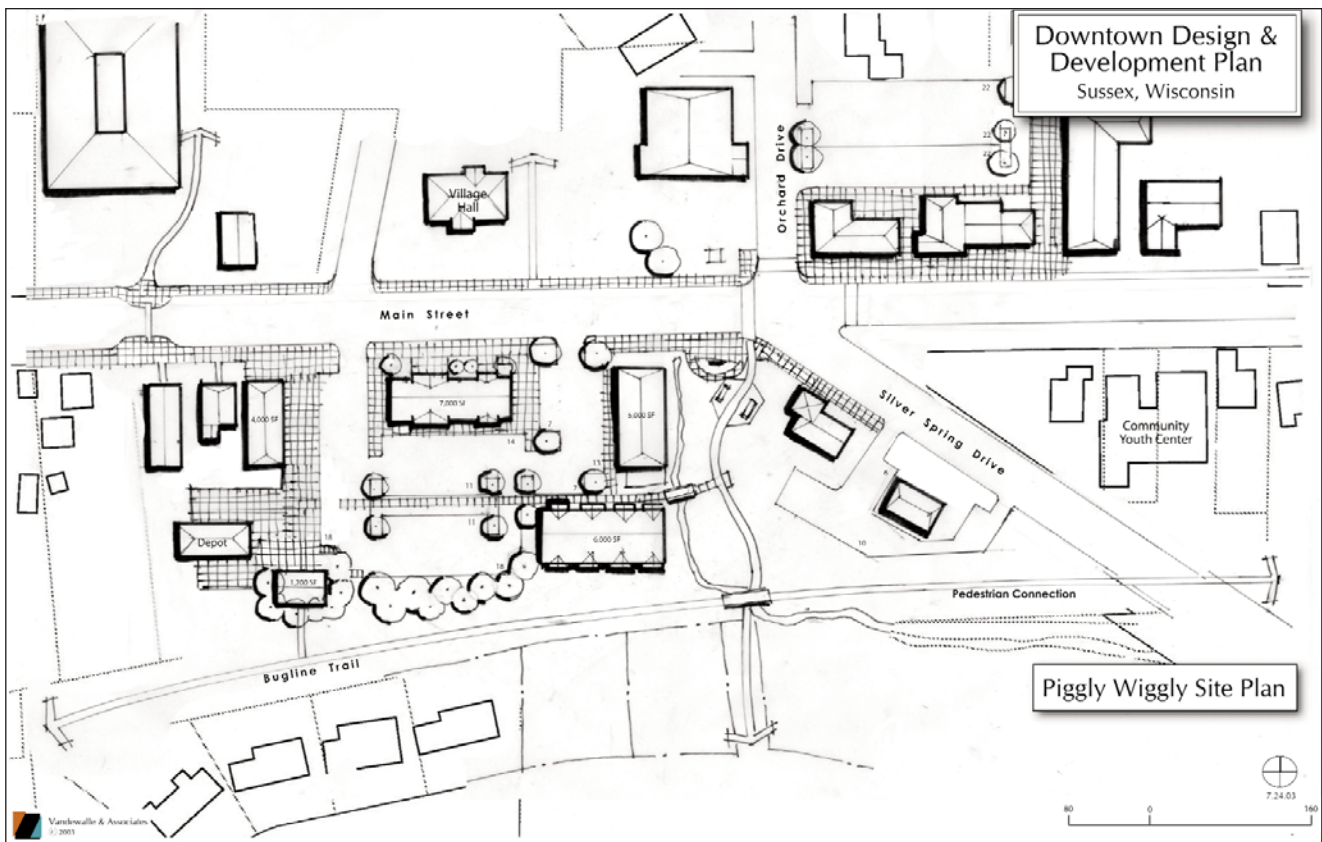
- Unified complex of commercial development
- Revitalized Main Street frontage
- Orientation to corner park and creek
- Integration with existing Main Street businesses
- Connection with Bugline Trail

### Guidelines

- Setback at Main Street: Minimal (i.e. five to fifteen feet)
- Setback at Creek: (Minimum required setback)
- Materials: Brick field, Limestone or pre-cast trim
- Style: Sloped roof
- Signage: Integrated with building
- Parking: At rear, coordinated, 3/1000 square feet or less
- Service areas: Screened
- Align access with Village Hall entry drive
- Provide pedestrian access from Bugline Trail
- Provide pedestrian access from corner park

See specific guidelines for Retail and Commercial Lots, and Parking in Appendix A.

See 2011 Implementation Supplement for Detailed and Updated Concepts



# DOWNTOWN DESIGN AND DEVELOPMENT

## CANNERY CROSSING DISTRICT

The pending development of Mammoth Springs will effectively bring downtown closer to the Highway 74 corridor. It will help anchor the eastern end of Main Street, and connect the central business district to the principal north-south highway serving the area.

New development in this sub-district should contain a compact mixture of retail and residential uses that build on the existing road-house flavor of the area, and help announce the presence of downtown. New buildings should provide a strong edge to the street through shallow setbacks and corner buildings that bracket the intersection. The intersection should be tied to the heart of downtown through a consistent streetscape theme, including directional signage to the center of Main Street. A new trailhead to the Bugline, and connections to the rail yards area, should also be incorporated into future redevelopment.

Over the long term, the east side of Waukesha Avenue between Silver Spring Drive and Main Street, should be redeveloped in a complementary manner. A mixture of residential and commercial uses, arranged in a more compact and interconnected pattern, would provide a stronger approach to this key intersection and to the downtown.

The existing properties to the northwest of the intersection of Waukesha Avenue and Main Street are an important asset to downtown Sussex. Many of the properties are currently underutilized. Its high visibility from Highway 74 makes this a valuable commercial property, with the potential to strongly influence the character of downtown. This represents an opportunity to create a quality commercial and residential redevelopment project at the western gateway to downtown Sussex. The following strategies and guidelines should guide the future redevelopment of this site. (See 2011 Implementation Supplement for more detailed and updated concepts.







## Northwest

- Assemble underutilized properties
- Integrate Bugline Trail and trailhead
- Redevelop corner site (potential bike shop)
- Encourage new streetfront commercial development
  - Implement-related business
  - Restaurant, Diner
  - Bicycle sales and service
- Coordinate and share parking
- Encourage multifamily residential (internal)
- Match existing Main Street streetscape
- Manage storm water

## Cannery Crossing District

### Potential Themes

- Templeton
- Bicycle/recreation
- Rural industrial
- "Roadhouse"
- Rail (Existing and Bugline)

## Northeast

- Coordinate with redesign of Highway 74
- Preserve existing restaurant at corner
- Explore rail-related retail

## Southeast

- Coordinate with redesign of Highway 74
- Assemble & redevelop underutilized properties

## Downtown Design & Development Plan

Sussex, Wisconsin





# DOWNTOWN DESIGN AND DEVELOPMENT

## GATEWAY SITE AT TEMPLETON CORNERS

### Strategies

- Currently underutilized properties
- Unified complex of mixed-use development
- Revitalized Main Street frontage
- Integration with Bugline Trail
- Relationship with Mammoth Springs

### Guidelines

#### Commercial sites:

- Setback: Minimal (i.e. five to fifteen feet)
- Materials: Brick field, Limestone or pre-cast trim
- Height: Two stories
- Style: Sloped roof
- Signage: Integrated with building

- Parking: At rear, coordinated, 3/1000 square feet or less

- Gas station corner treatment: Wall, signage, landscaping

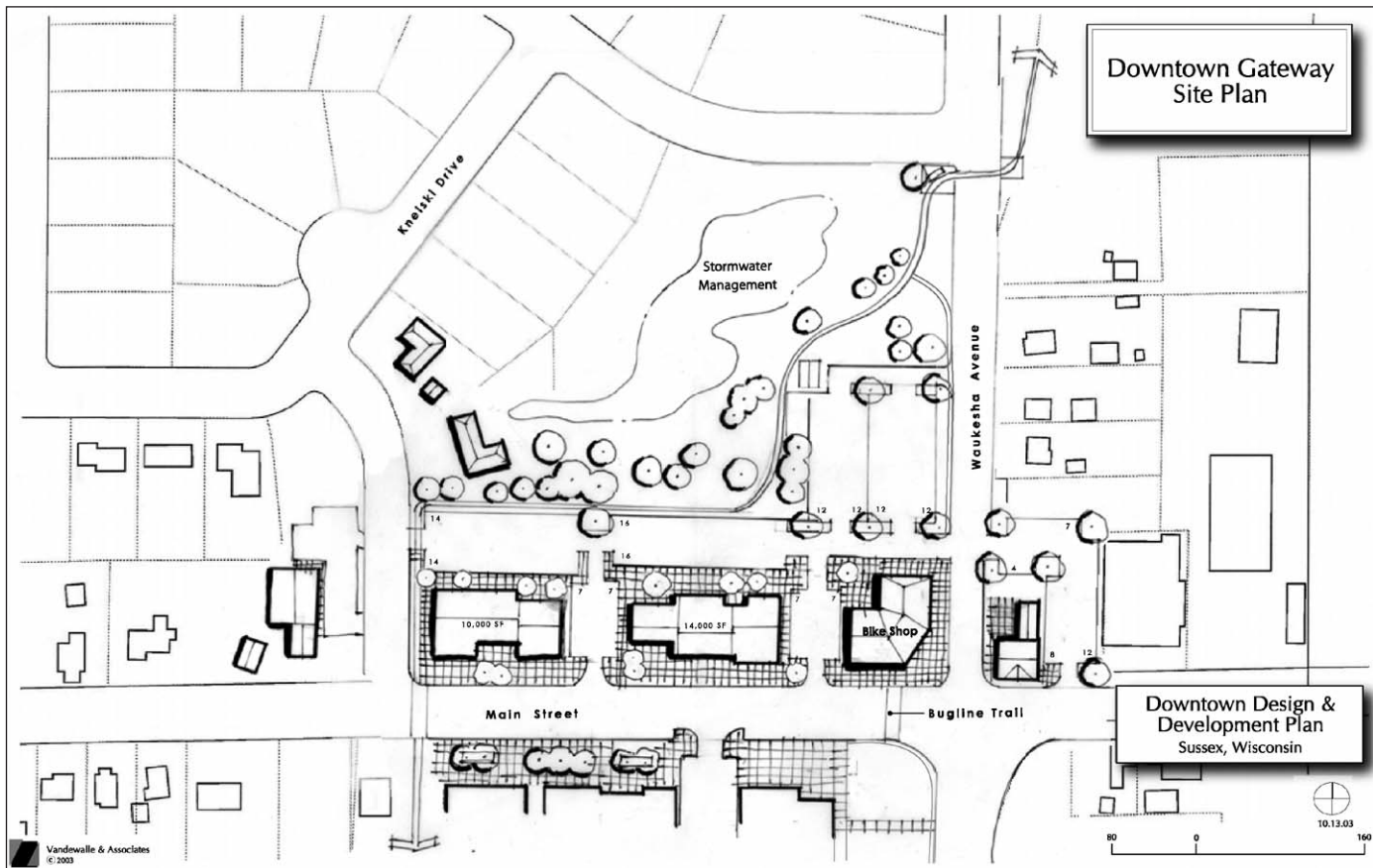
- Service areas: Screened

#### Public sites (Bugline Trail):

- Height: One-story information facilities
- Materials: Brick field, Limestone or pre-cast trim
- Style: Sloped roof
- Parking: Screened with landscaping

See specific guidelines for Commercial and Retail Lots, Store Fronts, and Parking in Appendix A.

See 2011 Implementation Supplement for Detailed and Updated Concepts



# DOWNTOWN PUBLIC IMPROVEMENTS

**P**ublic improvements will play an important role in the health of downtown. They provide the necessary infrastructure to support the functions of the community and the amenities to enhance its livability. Village improvements will:

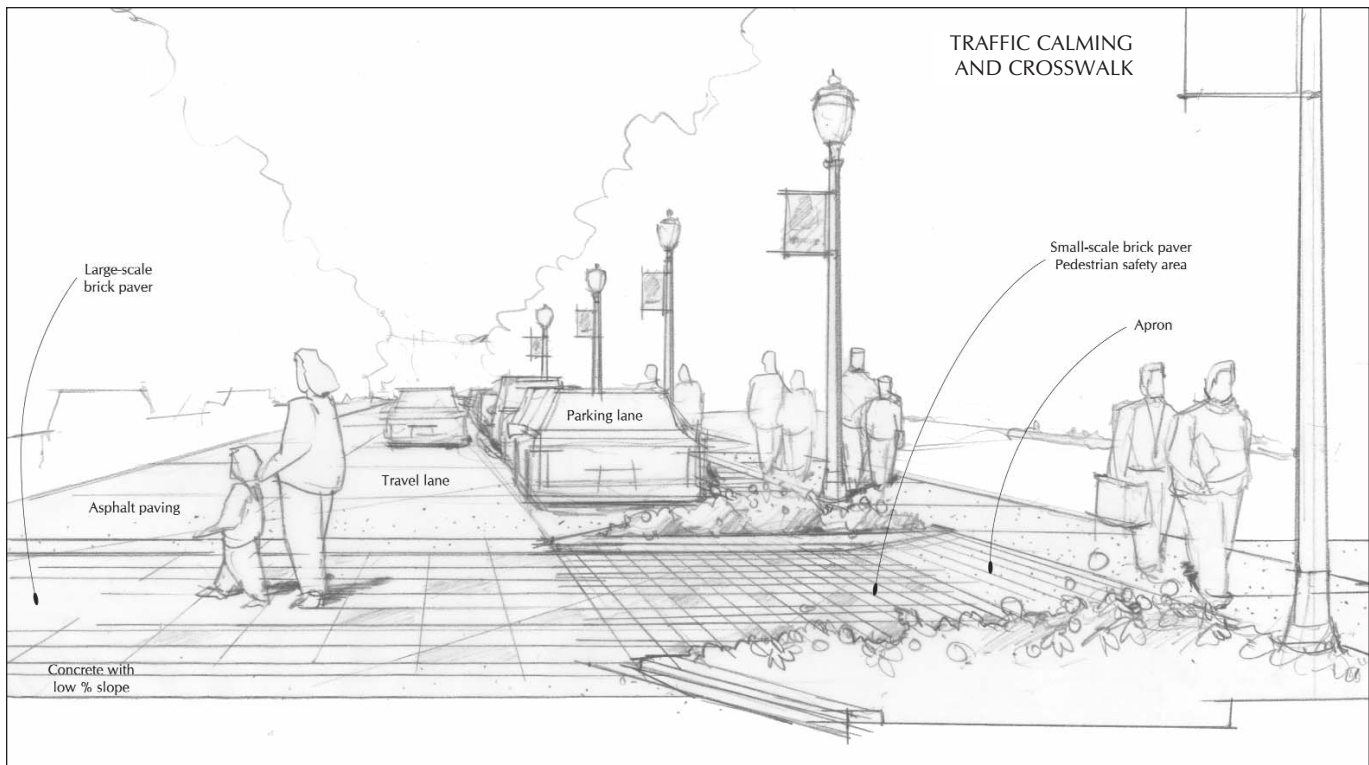
- Unite the downtown districts
- Connect the downtown to surrounding residential neighborhoods
- Create an identity for Main Street
- Enhance pedestrian safety
- Welcome out-of-town visitors

Village public improvements will include street infrastructure improvements, pedestrian and bicycle pathways, streetscape improvements (including street furniture), signage, and park improvements.

## STREET RIGHT-OF-WAY IMPROVEMENTS

### Street Improvements (Main Street)

- Install contrasting paving materials and/or striping to indicate on-street parking areas. Emphasize separation of parking areas from travel lanes.





# DOWNTOWN PUBLIC IMPROVEMENTS

## Traffic Calming and Crosswalk Improvements (Main Street and Waukesha Avenue)

- Accent Main Street pedestrian cross-walks.  
Add pedestrian signals.
  - Maple Avenue
  - Silver Spring Drive
  - Waukesha Avenue
  - Sussex Public Library
  - Proposed crossing of the Bugline Trail at Orchard Drive
  - Youth Center
- Reconstruct Bugline Trail crosswalks using change in material.
  - West side near Public Safety Building
  - East side near trailhead
- Install pedestrian islands at Main Street and Silver Spring Drive

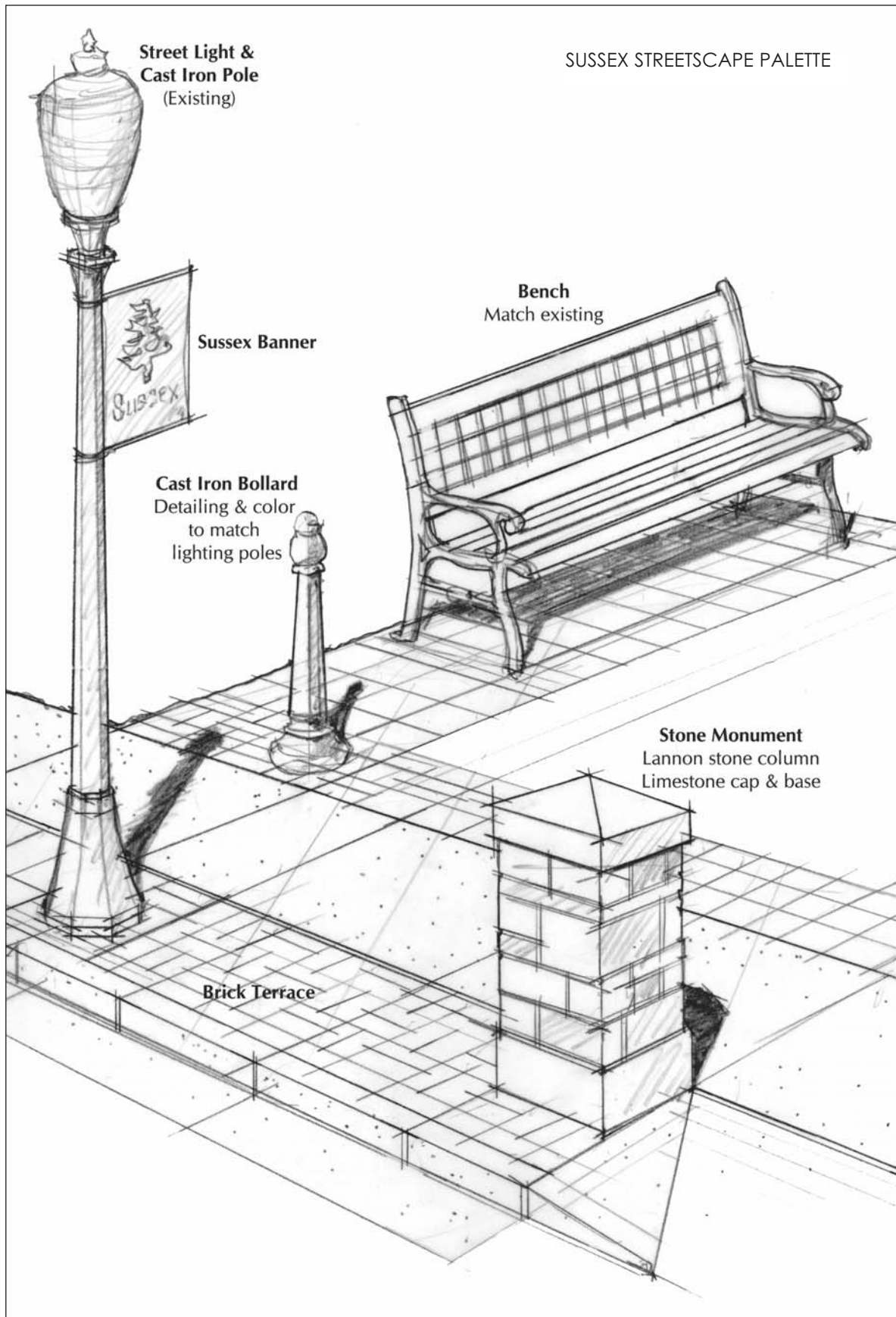
- On street light poles
- Also use on poles and bracket mounts on buildings
- Stone Monument (Lannon stone to match existing)
  - Use as base for
  - Use as features at pedestrian gateways
- Bench (to match existing)
  - Locate at pedestrian waiting and watching areas
- Bollard (to match existing cast iron)
  - Use to indicated separation between pedestrians and vehicles
- Concrete curb and gutter
- Concrete sidewalk

## SUSSEX STREETSCAPE PALETTE

A consistent use of materials and streetscape furniture throughout downtown will help to create a strong image for downtown, and help to visually unite the area despite diversity in age and style of existing buildings. Use the following palette of materials and street furniture in all public areas and in existing and new projects.

- Street Light/Pole (Pedestrian-scaled)
  - At regular intervals (to match existing) on Main Street
  - At intersections of pedestrian paths
- Banner (Sussex color and logo)

# DOWNTOWN PUBLIC IMPROVEMENTS



# DOWNTOWN PUBLIC IMPROVEMENTS

Streetscape Improvements (Main Street, Silver Spring Drive, and primary intersections)

- Install “primary streetscape amenities” (brick terraces between curb and sidewalk, period pedestrian-scaled street lights to match existing, and banners to match existing) on Main Street.
  - Between Maple Ave. and Silver Spring Dr.
  - Maple Avenue and Silver Spring Drive Intersections
  - Between Orchard Drive and Waukesha Avenue
  - Orchard Drive and Waukesha Avenue Intersections
  - Parking
- Install “primary streetscape amenities” (brick terraces between curb and sidewalk, period pedestrian-scaled street lights to match existing, and banners to match existing) on Maple Avenue.
  - Between Main Street and Hickory Drive
- Install “secondary streetscape amenities” (period pedestrian-scaled street lights to match existing, and banners to match existing) on Main Street.
  - Between the fire station and Maple Ave.
- Install Lannon stone pylons in terrace area at mid-block, traffic-calming and intersection crosswalks.
- Install seating areas at the intersections of traffic calming crosswalks and Main Street sidewalks. Seating areas should be placed outside of the flow of walkways, and be furnished with benches, trash receptacles, and pedestrian-scaled lighting.

## BICYCLE AND PEDESTRIAN NETWORK

Bugline Trail

- Complete Bugline Trail. Match existing trail type and quality.
  - Between Silver Spring Drive and Trailhead at Waukesha Avenue
- Create trail “nodes” where new connections are made to the Bugline Trail. Amenities could include a pull-off area, wayfinding signage and map, and benches.
  - Westchester Drive
  - County VV
  - Oak Drive
  - Maple Avenue
  - Depot Museum
  - “Point” Park
  - Silver Spring Drive
  - Mammoth Springs Development
  - Trailhead at Waukesha Avenue





# DOWNTOWN PUBLIC IMPROVEMENTS

Pedestrian and bike pathways and connections

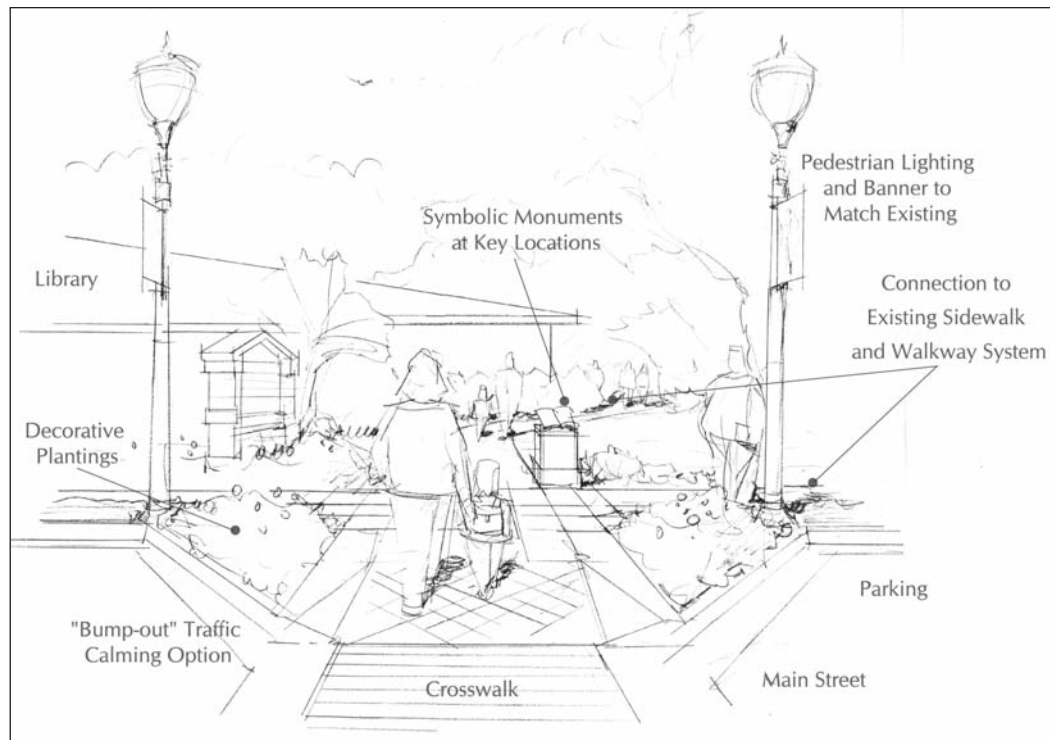
■ Designate on-street bike routes and pedestrian routes. Where warranted for traffic separation from bicycles, create designated bike lanes. Where warranted for traffic separation from pedestrians, construct sidewalks or roadside pathways. (See "Proposed On-Street Collectors" on Circulation Improvements plan.)

- Westchester Drive
- Locust Drive
- Oak Drive
- Ash Street
- Maple Avenue
- Elmwood Avenue
- Park Court
- Pembroke Street

- Silver Spring Drive
- Orchard Drive
- Pine Terrace
- Hickory Drive
- Ivy Avenue
- Outer Circle Drive

■ Construct shared bicycle and pedestrian path. (See "Proposed Bicycle/Pedestrian Path" on Circulation Improvements plan.)

- Between Ivy Avenue and Bugline Trail
- Between Linda Drive and Orchard Drive
- Between Linda Drive and Outer Circle Drive
- Between Main Street and Bugline Trail
- Between Hillview Road and Main Street
- Between Spring Green Park and Bugline Trail



# DOWNTOWN PUBLIC IMPROVEMENTS

## SIGNAGE

### Welcome

- Install downtown welcome signage and feature. (See “Downtown Gateway” on Circulation Improvements plan.)
  - Public Safety Building (west side) on County VV
  - Crest of hill (west side) on County VV
  - Bugline Trail on Silver Spring Drive (County VV)
  - Crest of hill (east side) on Main Street

### Wayfinding

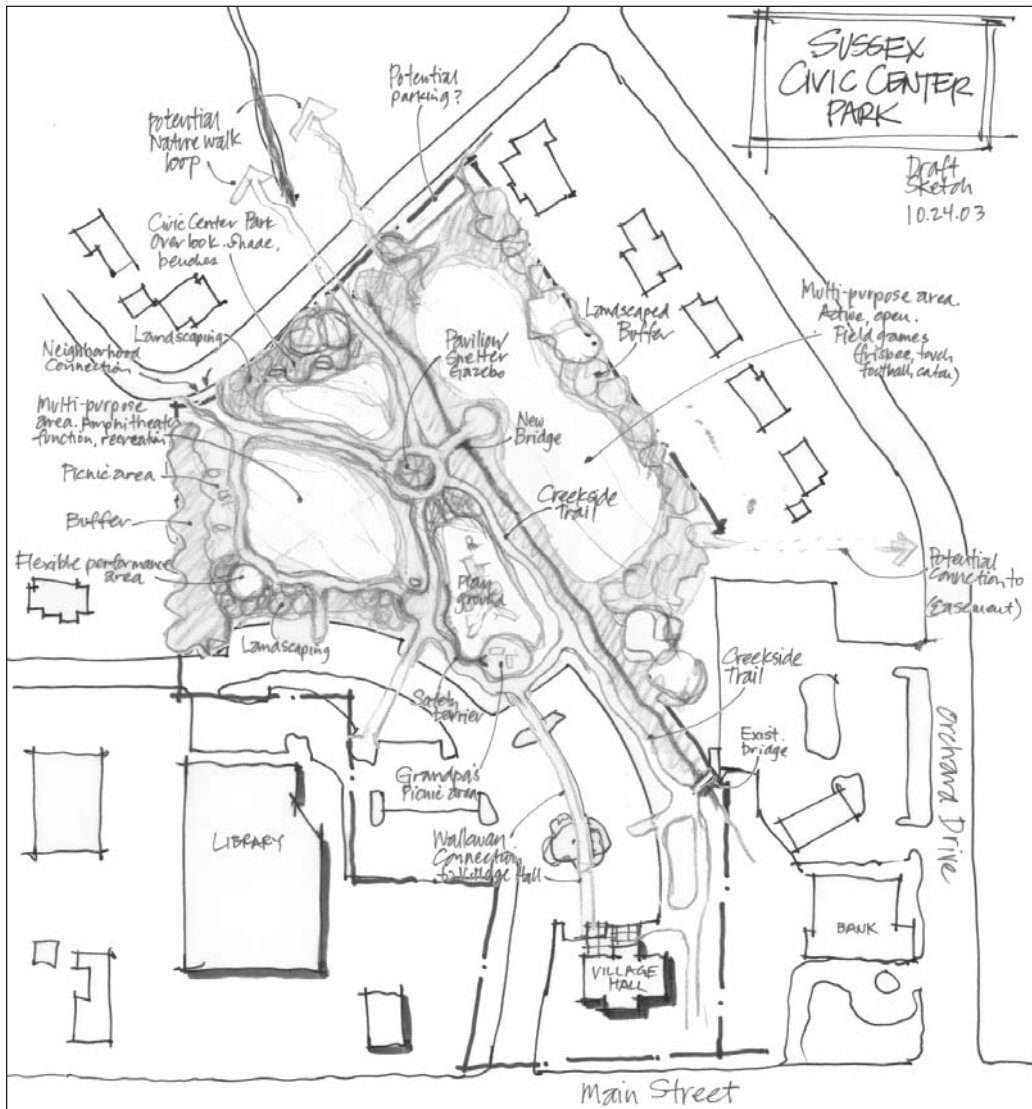
- Install downtown wayfinding signage (see “Wayfinding Information” on Circulation Improvements plan.)
  - Public Safety Building (west side) on County VV (east bound)
  - Crest of hill (west side) on County VV (east bound)
  - Maple Avenue approaching Main Street (north and south bound)
  - Silver Spring Drive approaching Main Street (northwest bound)
  - Main Street approaching Silver Spring Drive (west bound)
  - Waukesha Avenue approaching Main Street (north and south bound)
  - Main Street (Highway 74) approaching Waukesha Avenue (west bound)

### Pedestrian and Trail

- Install downtown wayfinding signage. (See Circulation Improvements plan.)
- Bugline Trail (near Maple Avenue, Silver Spring Drive, and Waukesha Avenue)
- Major pedestrian and bike connections to downtown



# DOWNTOWN PUBLIC IMPROVEMENTS



## PARKS AND OPEN SPACE

### Sussex Civic Center Park

Located at the heart of downtown and adjacent to the Village Hall and Public Library, the Civic Center Park is Sussex's central park. It should remain an important public open space for community gatherings, play, and relaxation.

- Maintain existing playground and adjacent picnic area. Add safety barrier between the playground and parking lot.

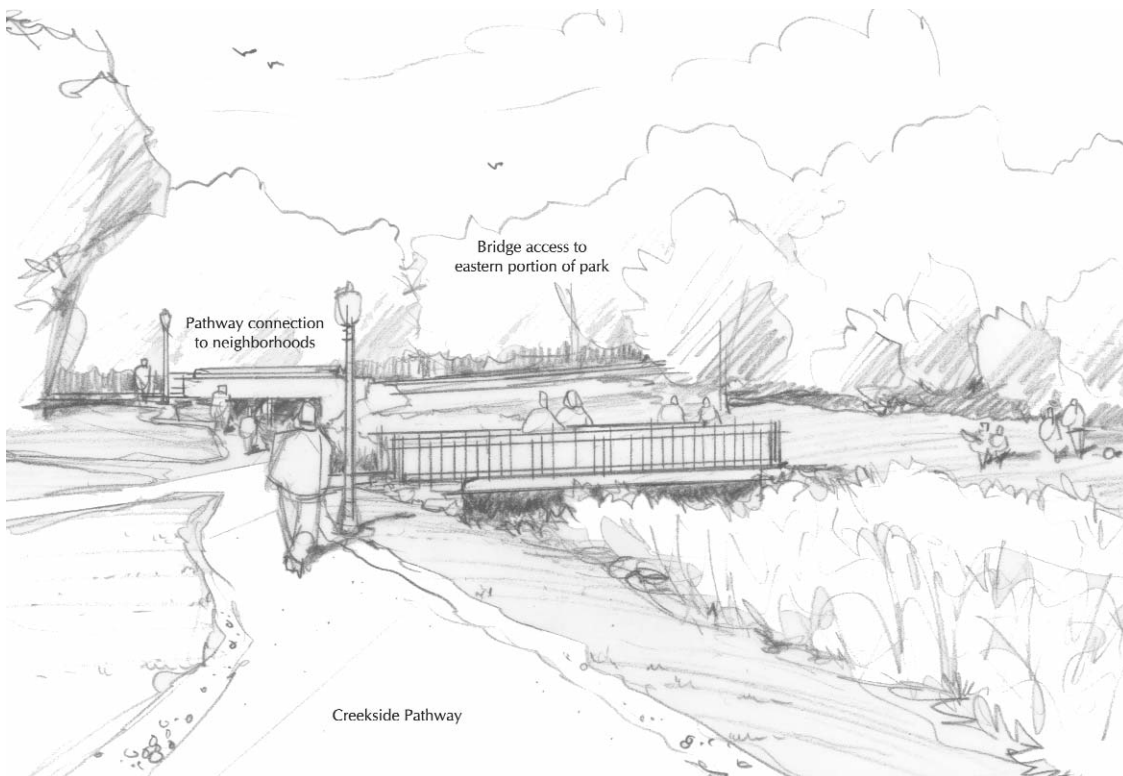
- Create pathways within the park connecting to adjacent neighborhoods and the Library. Connect to Village Hall through existing parking lot and tree island. Include a creek-side trail connecting Main Street and neighborhoods to the north.
- Construct a park shelter which may be open or include public rest rooms, water, electrical, etc. Also construct an information kiosk at a key pathway intersection.



- Create an evenly graded, slightly sloped, multi-purpose area to be used for public gatherings, performance events, and field recreation (located at the existing central open area). Also create a flexible performance stage at the corner of this area.
- Create an additional multi-purpose area east of the creek for informal and organized field recreation. Construct bridge over creek for access to eastern portion of park.
- Enhance edges of park with seating and landscaping. Enhance buffer at edges adjoining residential lots.

#### PUBLIC PARKING

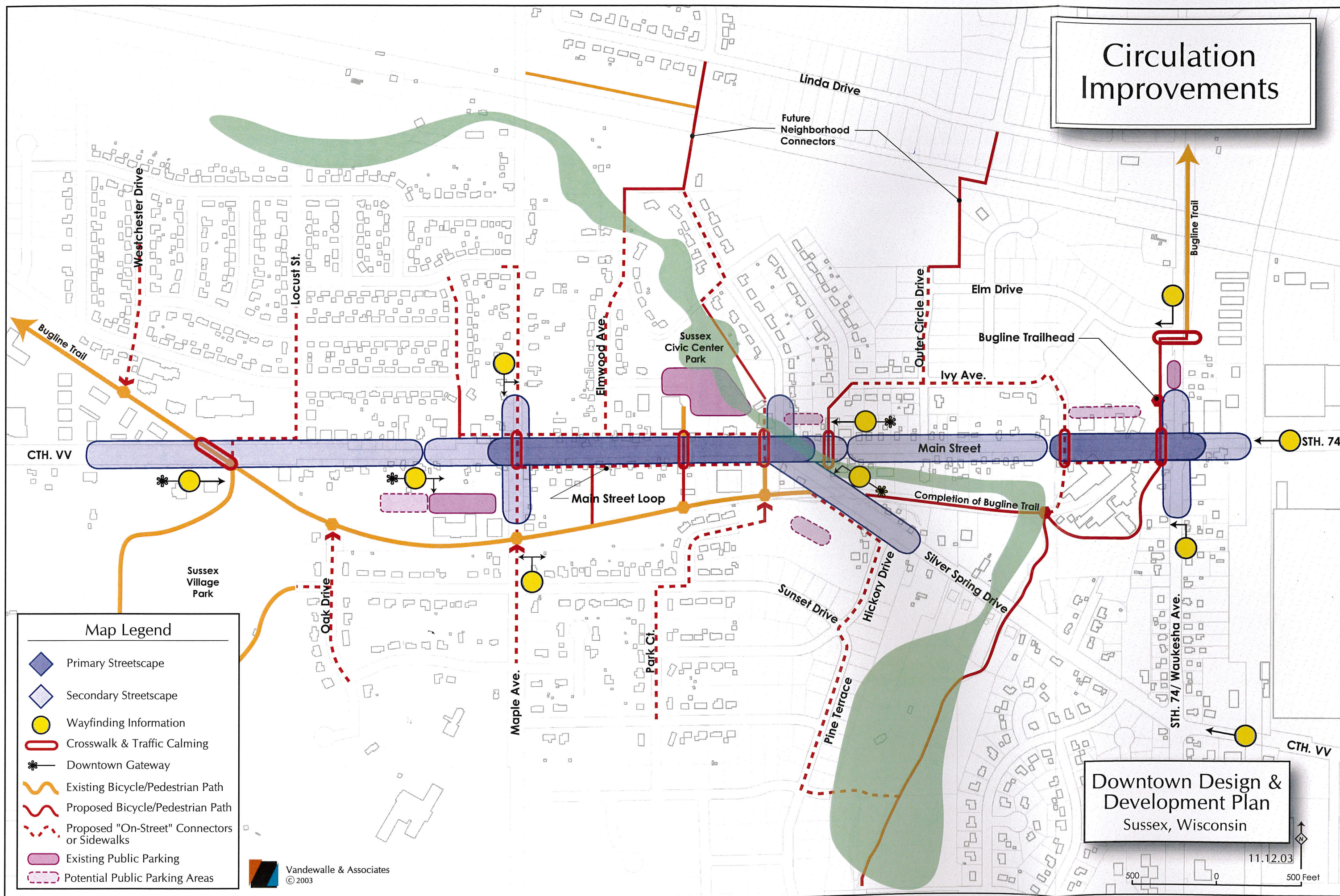
- Introduce/expand public parking lots in each of the three special districts (see graphic on page 41).
- Make sure that these lots are well landscaped and accessible (see specific guidelines for Parking in Appendix A.
- Preserve as much on-street parking as possible. Explore angled parking along one side of Main Street.







# Circulation Improvements



## Map Legend

-  Primary Streetscape
-  Secondary Streetscape
-  Wayfinding Information
-  Crosswalk & Traffic Calming
-  Downtown Gateway
-  Existing Bicycle/Pedestrian Path
-  Proposed Bicycle/Pedestrian Path
-  Proposed "On-Street" Connectors or Sidewalks
-  Existing Public Parking
-  Potential Public Parking Areas

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Downtown Design &  
Development Plan  
Sussex, Wisconsin

11.12.03  
500 0 500 Feet





# PLAN IMPLEMENTATION

# MAKING IT HAPPEN

Implementation of the plan will require several regulatory, capital improvements, and private sector actions including the following:

- Adopt the plan as an addendum to Comprehensive Plan.
- Reaffirm downtown as one of the "smart growth" planning areas required under that plan.

## PROJECT PRIORITIZATION AND TIMING

The selection and timing of design-related projects should generally be influenced by the following factors:

- The opportunity to piggy-back a project on other projects - both public and private
- The project's potential to tie together existing attractions and activity areas
- The ability of the project to spawn desired private investment on underutilized sites
- The cost of the project and the availability of funding sources
- The project's ability to remove and reverse dangerous and/or unsightly conditions in heavily used and highly visible areas

Using these criteria it seems that the Village's initial focus should be on the Templeton Corners area and the section of Main Street between Silver Spring and Maple. The reconfiguration of the Highway 74 and Main intersection, along with the Mammoth Springs development in particular, offer opportunities to leverage planned improvements and to get them at least partially funded

through negotiated development review (especially where opportunities for alternative transportation may be present).

As a general rule, anytime a public right of way is torn up for expansion or maintenance an opportunity is presented to put it back together in an improved fashion. The demolition itself is a value-added (hard cost) that should be capitalized on whenever possible. The Village should enter into discussions with the County to program new streetscape improvements and connections to the Bugline Trail in the proposed improvements for County Highway 74. Transportation enhancement monies are specifically earmarked for such improvements.

The section of Main Street between Silver Spring and Maple, on the other hand, represents the heart of the downtown district. Public streetscape improvements, including traffic-calming measures, along this stretch will help reinforce this identity and link many of the district's existing attractions such as the Village Hall, Library, Sussex Center Park, and the Bugline Trail. These improvements are needed to unify the loosely structured development pattern found here and elsewhere along the corridor.





# MAKING IT HAPPEN

These improvements will also tie together two of the special design districts identified in this plan, and 'wrap' them into a larger design image for the downtown. The need for traffic-calming measures are especially acute along this stretch of Main Street as a way to set-off this area from the rest of Main Street and to tie together the features previously mentioned.

Generally, public streetscape improvements in commercial districts are the types of projects best paid for through tax increment financing (TIF). This is because they can help spawn new private investment and tax base that can service the debt incurred from the improvements. Although grants for this work should be sought, the Village, given its current inability to use TIF, may consider postponing many of these improvements until the current TIF is retired. Another option is the creation of a Business Improvement District (BID) to fund special projects in the downtown. Meanwhile any contemplated general fund expenditures for public improvements may be better spent on projects off of Main Street such as trail connections, park improvements, and property/easement acquisitions which are harder to fund



under TIF due to the lack of direct or immediate impact on adjacent property values. A list of potential grant funds for trail projects is included in Appendix B.

Although streetscaping and traffic-calming are both important aspects of making downtown a more pleasant environment, the private redevelopment of underutilized parcels and the creation of a tighter downtown district may not reach full fruition until the issues of parking and land assembly are resolved. The Village should begin to explore the possibility of acquiring property for consolidated redevelopment and public parking at select locations identified in this plan. The availability of off site parking, combined with reduced parking standards, will maximize Downtown's redevelopment potential, thus creating a more tightly knitted, pedestrian-focused downtown and a more concentrated tax base. Private redevelopment projects on publicly acquired land, on the other hand, are ideal TIF projects since their subsequent sale provides a built-in tax revenue stream, and because they can involve special design covenants attached to a public-private development agreement.

## ZONING AND DESIGN STANDARDS

### Recommended Zoning Changes:

- Consider adding maximum dimensional standards and volumes for buildings in the B-4 Central Mixed Use district to limit their overall scale and mass. For instance, a maximum single floor square footage of 5000 square feet.

- Consider minimum and maximum lot coverage ratios that reflect the existing pattern, spacing, and ratios of indoor to outdoor space. (e.g. 60% minimum, 80% maximum building coverage)
- Consider maximum parking ratios that reflect urban rather than suburban standards (i.e. one space for every 400 square feet of commercial floor space.)
- Consider requiring minimum building heights of at least two stories.
- Consider changing "off-street parking and loading" to a conditional use tied to specific downtown design standards.

#### ADDITIONAL DESIGN GUIDELINES

- Consider adding a section to the Village's Design Guidelines that specifically addresses the downtown area:
- Include a standard that prescribes a minimum ratio of openings (i.e. windows and doors) to solid (opaque) surfaces for all new building facades.
- Incorporate (codify) additional site-specific design guidelines discussed elsewhere in this plan.
- Cross-reference B-4 zoning requirements and downtown design guidelines in both documents.

#### DEVELOPMENT INCENTIVES

- Consider granting zoning density bonuses or tax abatements in exchange for the dedication of public easements to Bugline Trail, or the private development of public streetscape improvements.
- Encourage outdoor seating and displays to help enliven Main Street.

#### OTHER ACTIONS

- Retain control over east end of Main Street in order to exercise greater local control over traffic-calming and on-street parking.
- Program public streetscape improvements into a Capital Improvements Plan coordinated with anticipated private development and other public improvements.
- Work to consolidate parking in a few public or shared lots rather than on numerous individual sites.

## FUNDING OPTIONS

Funding for public streetscape improvements could come from a variety of sources other than tax increment financing (TIF). Besides general revenue, these may include or involve the creation of a Business Improvement District (BID), or through grant solicitation. Of the latter, Federal TEA-3 transportation enhancement funds may be promising due to the relationship between downtown and the Bugline Trail.

A list of potential grant sources for public trail projects is included in Appendix B.





## APPENDIX A: SUPPORTING DESIGN GUIDELINES





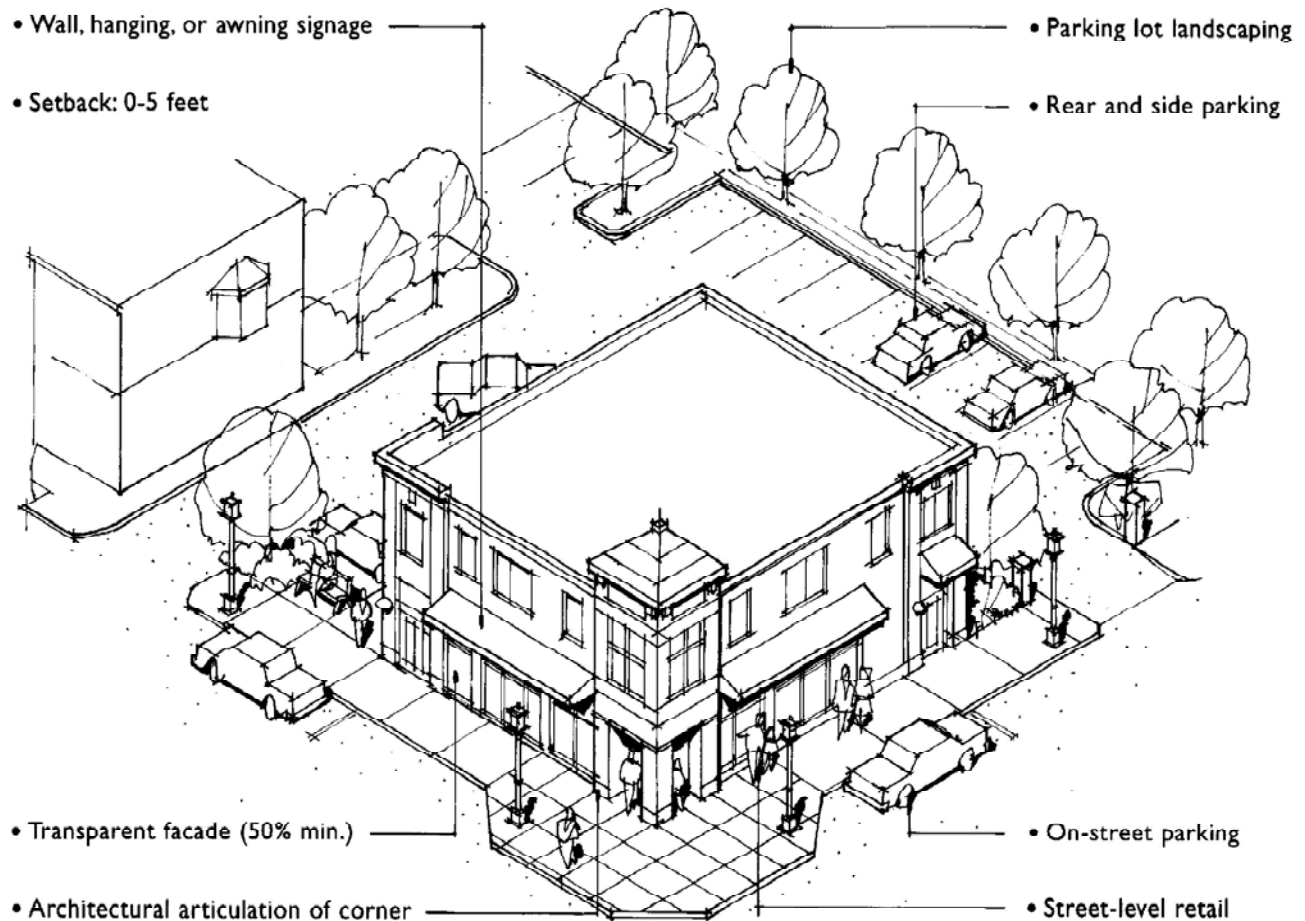
*Sussex*

## **Downtown Design Guidelines**

## Corner Retail Lot

The following guidelines are appropriate for corner commercial lots. Corner lots are important because they are highly visible and accessible. The guidelines are intended to help create efficient, viable, attractive, and pedestrian-scaled, corner commercial development.

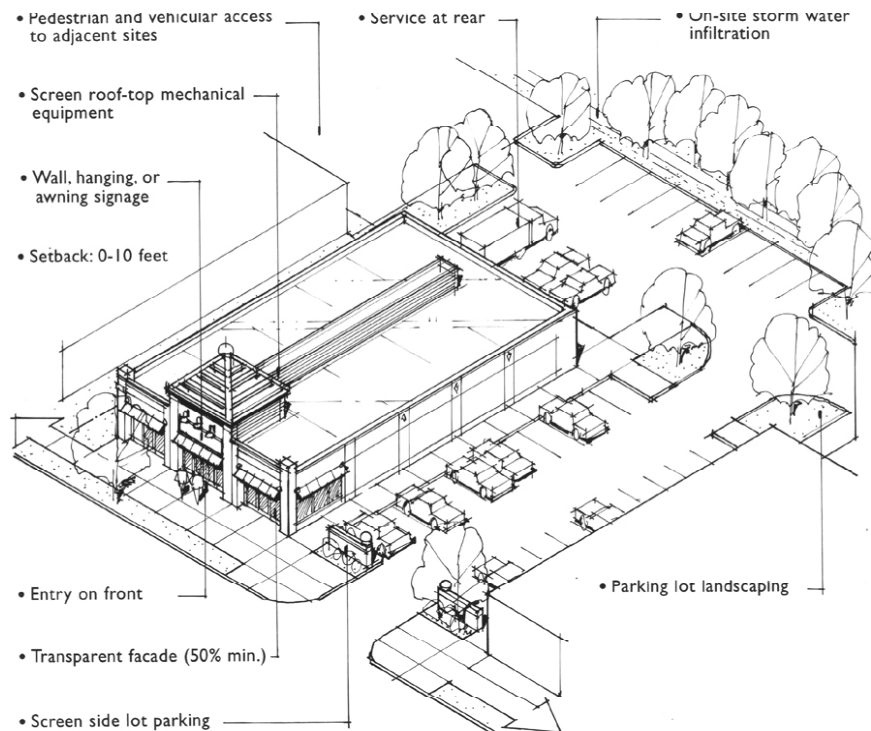
- Pedestrian-scaled streetscapes
  - Building presence on street
  - Convenient and coordinated parking and circulation
- 
- Minimum setback uses development land efficiently and brings front entries near the pedestrian-scaled public sidewalk.
  - Transparent facades increase visibility of business activity from the sidewalk and street. Integrated signage increases visibility of business name and reduces visual clutter of the streetscape.
  - Architectural articulation (projections, recesses, detailing, etc.) of the corner defines the edge of two streets and increases the memorability of the businesses and the structure.
  - Parking on the street and at the side and rear allows for convenient access by car.
  - Screening of parking lots and service areas minimizes negative impacts.



## Typical Commercial Lot

The following guidelines are appropriate for typical commercial lots. The guidelines are intended to help create efficient, viable, attractive, and pedestrian-scaled commercial development.

- Visible business information
  - Pedestrian-scaled and attractive streetscapes
  - Convenient and coordinated parking and circulation
  - Improved appearance through screening
- 
- Minimum setback uses development land efficiently and brings front entries near the public sidewalk.
  - Transparent facades increase visibility of business activity from the sidewalk and street.
  - Integrated signage increases visibility of business name and reduces visual clutter of the streetscape.
  - Access to adjacent sites increases accessibility to all businesses and reduces unnecessary duplicated drives and walks.
  - On-site storm water infiltration reduces runoff leaving the site reducing the burden on stormwater systems.
  - Screening of parking lots and service areas minimizes negative impacts.
  - Screened mechanical equipment improves appearance.

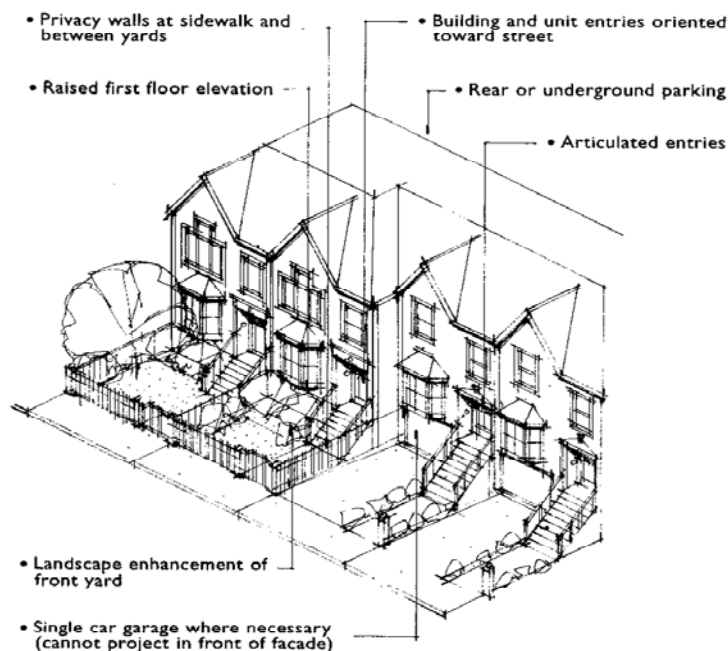




## Urban Residential

The following guidelines are appropriate where high-density, urban-scaled housing is desired. The guidelines are intended to help create efficient, secure, and pedestrian-scaled residential development. Two-story “town house” type units are shown.

- Efficient use of development land
  - Urban character
  - Pedestrian-scaled streetscapes
  - Opportunities for interaction between neighbors
  - Inviting residential units
  - Security and privacy
- 
- Minimum setbacks use development land efficiently and bring front entries near the public sidewalk.
  - Variation in setback dimensions avoid monotonous blocks, define individual units, and increase privacy and security.
  - Fences or walls at the sidewalk and between front yards create intimate pedestrian-scaled spaces and increase privacy and security.
  - Raised first floors increase privacy and security
  - Articulated (projections, recesses, detailing, etc.) entries identify individual units and add interest to the town house facades.
  - Parking, located under the first floors or at the rear, keep the street facade and front yard pedestrian-scaled.
  - Front garages, when they must be used, should be of minimum width and not project in front of the remainder of the facade.



## Building Elements: Detailing

*Detailing can help achieve a desired sense of scale and character as well as compatibility with adjacent buildings.*

### Performance Guidelines

- Create appropriate scale (pedestrian, auto, etc.) through detailing.
- Create continuity with adjacent buildings through detailing.

### Design Guidelines

- Consider using architectural elements from adjoining buildings to establish a sense of continuity.
- Building detailing including sills, headers, transoms, cornices, signage bands shall be compatible in design and elevation with those of existing buildings in the immediate area.
- Avoid large flat, blank surfaces without windows or architectural details, particularly at pedestrian levels.
- The infill structure should reflect both the ratio of window to solid wall area and the size and proportion of windows and door openings of the surrounding buildings.



## Building Elements: Storefront

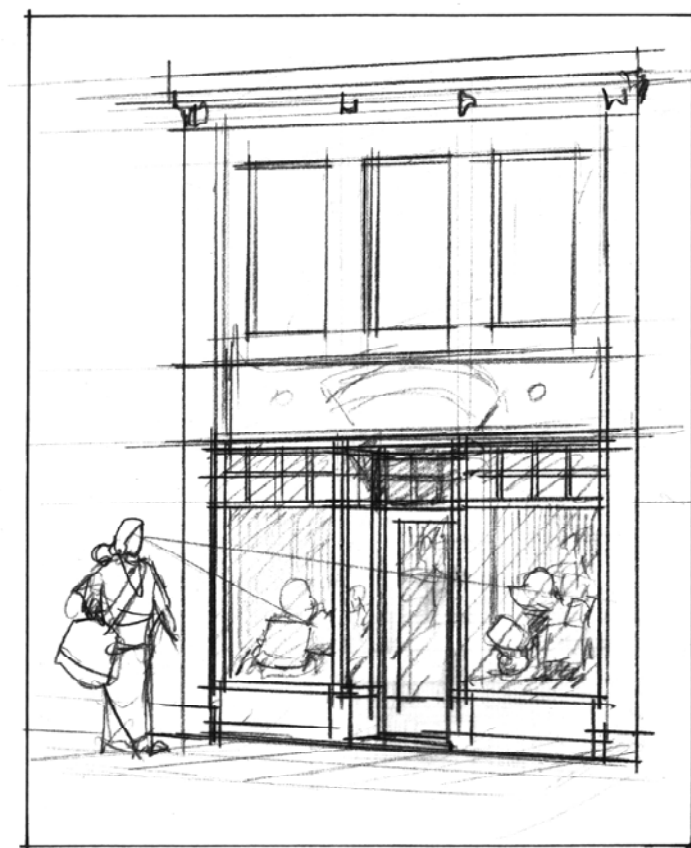
*The storefront is the most important element of a commercial facade. The design of the storefront affects the perception of the business, the functional interaction of the business and the public, and the character of the street.*

### Performance Guidelines

- Building storefronts should be inviting to the public.
- Activity and displays inside the place of business should be visible to the street.

### Design Guidelines

- At least 75 percent of the first floor street-side facade should be transparent (through the use of display windows).
- Establish a consistent first-floor height (storefront height) to establish a sense of scale for pedestrians.
- Rhythm of store floor should harmonize with rhythm of upper stories.



## Building Appearance: Materials

*The materials of a building help to determine how well the structure blends in with its neighbors.*

### Performance Guidelines

- Reflect existing palette and local traditions.

### Design Guidelines

- The new building should utilize the predominant colors and materials of the area.
- Use clear or slightly tinted glass; do not use mirrored, smoked, heavily tinted glass.
- Warm, small scale materials should be used on pedestrian accessible surface.
- Newer materials such as concrete block, poured in place concrete should not be used as finish material on front or visible facades.
- Asphalt shingle siding should not be permitted.
- CBD stone or brick facing should be of even coloration and consistent size.
- Cinder block, concrete block, concrete slab, or concrete panel should not be permitted.



## Building Appearance: Color

*The colors of a building are a large factor in determining how well the structure blends in with its neighbors.*

### Performance Guidelines

- Minimize discordant use of color.
- Maximize color harmony within and between properties.

### Design Guidelines

- Exterior colors shall be compatible and harmonious with existing buildings.
- Exterior color schemes shall be used consistently throughout the property, including on the both the upper and lower portions of buildings.
- Color combination schemes shall be limited to no more than three different colors for all the structures on the property.

## Building Appearance: Utilities

*Utilitarian features, if not integrated into design considerations, can detract from the site and building.*

### Performance Guidelines

- Minimize negative visual impact of utilitarian features.

### Design Guidelines

- Avoid cluttering facades with brackets, wiring, meter boxes, antennae, gutters, downspouts, etc.
- Place roof-top utilities out of view from public rights-of-way through proper location on roof or site or through screening.

## Signage: General

*Signs produce a lasting impression and an indication of the commercial health of a business district. One of the most important determinants of the visual character and coherency of communities is signage.*

### Performance Guidelines

- Ensure that signs aid in orientation and adequately identify uses and activities to the public.
- Discourage excessive visual competition in signage.
- Reduce distractions and obstructions from signs.
- Preserve or enhance Village character by requiring new and replacement signage that is:
  - creative and distinctive
  - compatible with the surroundings
  - appropriate to the type of activity to which it pertains
  - expressive of the identity of individual proprietors or the community as a whole
  - appropriately sized in its context, so as to be readable



## Signage: Type

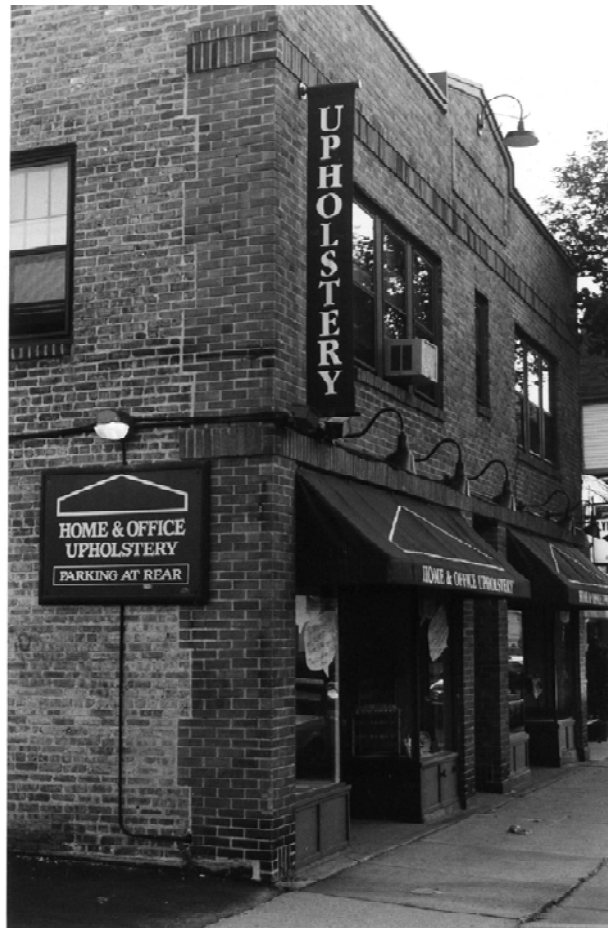
*Signage types have a large impact on the appearance of a commercial districts.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- The following signs may be used for individual businesses (see additional guidelines):  
wall signs, projecting signs, and awning signs.
- Signs painted directly onto a window surface can often be quite effective. Appropriately designed neon window signs (with custom shapes and colors complementary to the building) are another option, and can add character to an establishment if sensitively handled.





## Signage: Location

*Consistency in sign location between businesses will influence visibility of signs, conflicts between signs, and integration with architectural character.*

### Performance Guidelines

- Increase readability of all signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- Signs should be concentrated near the pedestrian level in storefront areas or in the "information" or "signage" band above the storefront.
- Locating signs on the upper facades of buildings should be avoided in order to avoid visual clutter and obstructing important architectural features. Use of a conservative, well-designed banner or sign, meeting all other guidelines and well integrated with architectural elements, may be approved on an individual basis.
- Location of signs shall be integrated with architectural elements.
- Signs identifying commercial establishments should generally be placed within a long, continuous information band immediately above the storefront or should be applied directly onto the display window. The information band should generally be between 18 inches and 26 inches in its vertical dimension and must never be allowed to cover transom windows and other architectural details and elements.
- Signs on adjacent storefronts should be coordinated in height and proportion and, wherever possible should use the same sign format (or they should at least employ identical backgrounds).
- Wall signs should not extend higher than the eave line or top of the parapet wall of the principal building. Such signs should not extend above the lowest point of the roof, nor beyond the ends of the wall to which it is attached. No part of a wall sign, including the display surface, should extend more than 12 inches from the building surface.

## Signage: Number & Size

*Limitations on the number and size of signs for each business has a direct influence on the character of a community and its streets.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- The number of signs per building is determined by the following guidelines:
  - There should be no more than one sign per occupancy.
  - Each ground floor occupant of a building may display one sign.
  - Each occupant in an upper level of a building may display one sign.
- Information band should generally be between 18 inches and 26 inches in its vertical dimension and must never be allowed to cover transom windows and other architectural details and elements.
- Projecting signs shall be no larger than 12 square feet.
- Signs in the downtown should relate to pedestrians and people moving in slow moving vehicles. Large, auto-oriented signs (pole or pylon signs) are inconsistent with both the scale of the downtown and its pedestrian character and therefore, they should be avoided.
- Window signs should not exceed more than 30 percent of the window area in which they are displayed.
- Appropriate dimensions are relative to the sign type and its location and placement. Smaller, simply designed signs are the easiest to read, and therefore, the most effective. The signs must not obscure important architectural details or features.
- The total area of signs on a building wall shall not exceed 2 1/2 square feet per linear foot of building, not to exceed 25 square feet.

## Signage: Style & Lettering

*The style and use of lettering on signs will influence visibility of signs and the integration with architectural character.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Increase integration with architectural features and character.

### Design Guidelines

- Lettering styles should compliment the style and period of the building on which they appear. Traditional block and curvilinear styles that are easy to read are preferred. No more than two different type styles should be used on the same sign to avoid a cluttered appearance.
- Letters and symbols on the signs should conform to standards established throughout the downtown.
  - Individual letters or symbols may be attached to an awning, marquee, building surface, wall or signboard.
  - Letters or symbols should not project more than 12 inches from the building surface.
  - Such letters and symbols should not obscure the architectural features of the building to which they are attached.
  - Such letters and symbols should not extend above the lowest part of the roof, nor beyond the ends of the wall to which they are attached.
  - When a lot fronts on more than one street, the aggregate sign area facing each street frontage shall be calculated separately.

## Signage: Material

*Use of materials in signs has a strong influence on the overall character of a community's commercial district.*

### Performance Guidelines

- Sign materials should be consistent with, or at least complement the original construction materials and architectural style of the building façade on which they are to be displayed.

### Design Guidelines

- Natural materials such as wood and metal are much more appropriate than plastic. Internally lit plastic signs are out of context with the period and styles that are encouraged in the downtown and are often the most offensive type of signage in such areas.
- Permitted sign materials include glass, plastic, wood, brass, metal leaf, metal plates, canvas or related fabric, or etched glass, stone or concrete.



## Signage: Color & Illumination

*Consistency in use of color palettes and restricted use of illumination techniques can enhance the day and night time impression of a commercial district.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.

### Design Guidelines

- Illumination of exterior signage shall be limited to shielded spotlight.
- Flashing signs are not permitted.
- Sign color should adhere to the following guidelines:
  - Sign colors should be chosen to complement, not clash, with the facade color of the building.
  - Signs should normally not contain more than three colors, except in instances of illustration. Dark backgrounds with light colored lettering are preferred. Examples of preferred background colors are burgundy, red, forest green, chocolate brown, black, charcoal, and navy blue.
  - Preferred lettering colors are ivory, white, or gold. "Day glow" colors should be prohibited.
- The type of lighting that is most appropriate to the character of the downtown is direct illumination from a shielded source of light. Internal illumination is generally out of character for the area. Exceptions can be made, however, for contemporary "infill" buildings which, where internally lit signs with opaque backgrounds and glowing translucent letters may be permitted. Also, individual solid metal letters with internal lighting tubes that backlight the wall in a "halo" effect may also be used.
- Neon window signs may be permitted in cases where they are custom designed to be compatible with the building's historic and/or architectural character. Neon signs should meet the same dimensional requirements as other signs in the downtown.

## Signage: Wall

*Appropriate use of wall signs provides a functional and aesthetic addition to a business.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- Wall signs should not project more than 12 inches from the building surface.
- Such signs should not obscure architectural features of the building.
- Where a lot fronts on more than one street, the aggregate sign area facing each street frontage should be calculated separately.
- Where two or more wall signs are affixed to one wall, the gross display area should be the sum total area of all signs.
- Wall signs should not extend higher than the eave line or top of the parapet wall of the principal building. Such signs should not extend above the lowest point of the roof, nor beyond the ends of the wall to which it is attached. No part of a wall sign, including the display surface, should extend more than 12 inches from the building surface.

## Signage: Awnings

*Appropriate use of awning signs provides a functional and aesthetic addition to a business.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- Awnings: size, color and placement should complement the architectural character of the building.
- Soft, weather-treated canvas or vinyl materials which allow for flexible or fixed installation shall be used.
- Awning signs should be painted on or attached flat against the surface of the awning, but not extend beyond the valance or be attached to the underside.
- Letters on awning signs should not exceed 10 inches in height.
- A minimum of 7 feet above sidewalk level should be allowed for pedestrian clearance.

## Signage: Three Dimensional

*Appropriate use of three-dimensional signs provides a functional addition to a business.*

### Performance Guidelines

- Increase readability of everyone's signs.
- Decrease conflict between signs.
- Increase integration with architectural features and character.
- Decrease obstruction of architectural features.

### Design Guidelines

- Projecting signs, if flat, should not exceed 12 square feet.
- The total area of a three dimensional sign should be determined by enclosing the largest cross-section of the sign in an easily recognizable geometric shape and computing its area.
- The sign should be hung at right angles from the building and should project no more than 4 feet from the building or one-half of the sidewalk width, whichever is less.
- The supporting framework should be in proportion to the size of such sign.
- Signs that project over a public right of way (including sidewalks) should be covered by a public liability insurance policy, which names the community as the insured party.
- The top of the sign may be suspended in line with one of the following, whichever is the most successful application of scale, linear continuity, and visibility as determined by the Historic Commission:
  - Suspended between the bottom sills of the second story windows and the top of the doors and windows of the ground floor; or,
  - The lowest point of the roof of a one story building.
- A projecting sign should have a minimum clearance of 10 feet above grade when located adjacent to or projecting over a pedestrian way. If projecting over a driveway or alley, the clearance should be at least 15 feet.
- Free standing pole signs should not exceed 25 feet in height and 10 square feet in area.



## Amenities: Landscaping

*Well-placed use of landscape materials can enhance a site and its building.*

### Performance Guidelines

- Complement site and building design.
- Screen undesirable views.
- Provide shade.

### Design Guidelines

- One canopy tree shall be provided within, or within 5 feet of the edge of, on-site paved areas for every 2,000 square feet of paved area.
- All areas which are not covered by impervious paving or structures shall be covered with vegetative groundcover.
- Base of freestanding signs shall be concealed by plant material.
- The use of vegetation and various hardscape elements helps separate and contain pedestrians and vehicles to their respective areas.

## Amenities: Street Furniture

*Well-placed use of street furnishings can enhance a site and its building.*

### Performance Guidelines

- Provide safe, clean, functional site.

### Design Guidelines

- Provide benches where people need to wait or enjoy watching.
- Provide lighting at corners and walkways.
- Provide trash receptacles where easily maintained and where people wait.
- Exterior lighting shall be compatible and harmonious with the general design theme.
- Design, color, height, location, and light quality of on-site pedestrian scaled lighting shall be consistent with and complement the district's character.
- Design, color, height, location, and light quality of on-site vehicular scaled lighting shall be consistent with and complement the district's character.

## Amenities: Screening

*The impact of unavoidable undesirable views can be minimized through the use of landscape and hardscape screening techniques.*

### Performance Guidelines

- Minimize the impact of undesirable views.
- Maintain the pedestrian scale at the street level by continuing the street facade and masking the gap created by the drive and parking.

### Design Guidelines

- On-site paved areas, including parking lots, loading areas, circulation drives, and patios shall be screened from the view of the public rights-of-way and adjoining properties.
- Trash storage areas, air conditioning units, and related storage and utility areas and components shall be fully screened from the view of adjoining properties, public rights-of-ways and customer areas.
- When using hardscape elements, use materials that are similar in texture, material, and color to the materials which are used on the surrounding buildings.
- Commonly used buffers include the following:
  - Walls, fencing, & arbors
  - Upraised planters
  - Combination shrub/tree plantings

## Parking Location

*Parking is an integral component of downtown development and important to business accessibility. Public and private parking lots can, however, also detract from the appearance of the downtown.*

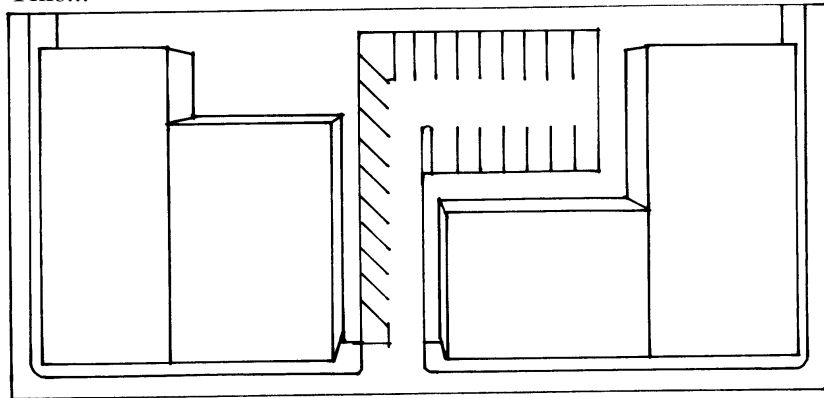
### Performance Guidelines

- Provide adequate parking for downtown businesses.
- Preserve the pedestrian-friendly nature of a downtown streetscape.

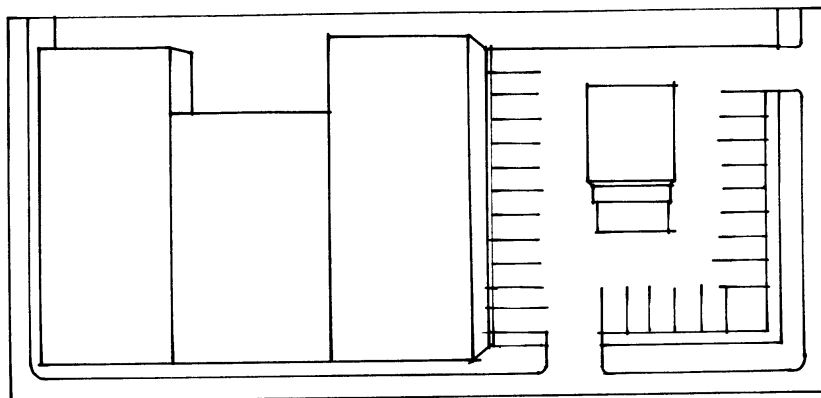
### Design Guidelines

- Parking lots should be sited at the rear or the interior side of the building in order to minimize gaps in the continuous building facades of the street.
- When parking lots are sited behind a building, it is preferred that they are accessed by an alley. This minimizes gaps that would otherwise be created by on-street driveways.

This...



Not This...





## Parking Buffering

*The visual impact of parking areas can distract from the appearance of a property or streetscape.*

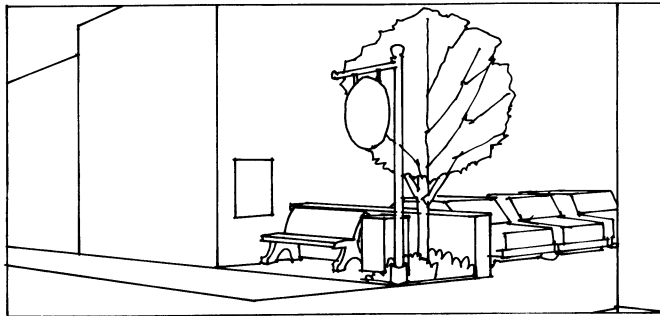
### Performance Guidelines

- Preserve pedestrian friendly nature of downtown streetscape.
- Separate pedestrians and vehicles.

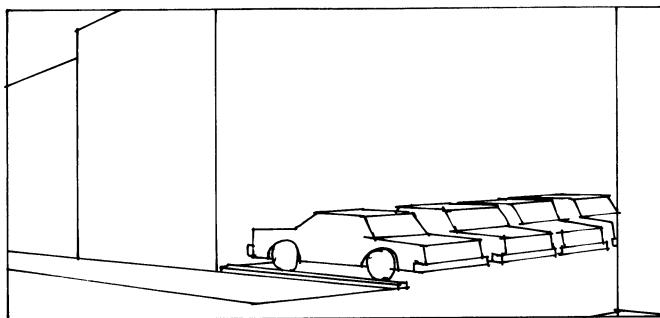
### Design Guidelines

- When it is necessary to place parking lots in-between buildings, rather than behind, they should be buffered from the street side by either architectural elements or plantings.
- Continue the face of the street facade by locating screening in line with adjacent facades.
- Commonly used buffers include the following:
  - Walls, fencing, & arbors
  - Upraised planters
  - Combination shrub/tree plantings
- When using hardscape elements, use materials that are similar in texture, material, and color to the materials which are used on the surrounding buildings.

This...



Not This...



## Building Location

*A primary consideration of redevelopment of a property is the location of the building on the site.*

### Performance Guidelines

- Maximize presence of building on street.
- Minimize impact of parking.
- Maintain existing enclosure of streetscape.
- Maximize pedestrian accessibility.

### Design Guidelines

- Locate building at streetside of lot with parking in rear or side.
- Minimize setback of street facade.
- Construct facades in alignment with neighboring building facades (CBD infill).
- On corner lots, place buildings (versus parking lots) at the corner of site. These buildings will also serve as landmarks and provide a sense of enclosure at intersections.
- Use building to screen parking lots.

## Public & Service Entry Locations

*The location of public and service entries to a facility can influence the functional and commercial success of the property.*

### Performance Guidelines

- Maximize accessibility of public entries.
- Minimize impact of undesirable views of service areas.

### Design Guidelines

- Locate pedestrian entry on street side of building.
- Locate service areas which require docking loading area at rear or side hidden from street.
- Provide additional entry where off-street parking is provided at rear or side.
- Service entries design should harmonize with building design through color, materials, and signage.



## Vehicular & Pedestrian Access

*How vehicles and pedestrians enter and leave the site is an important safety, functional, and appearance consideration.*

### Performance Guidelines

- Maximize ease of access and egress for pedestrians and vehicles.
- Minimize conflict between vehicles and pedestrians.

### Design Guidelines

- Minimize number of curb cuts.
- Allow for continuous flow of vehicles through site.
- Integrate pedestrian and vehicular circulation between adjacent buildings.
- Mark and sign areas of vehicular and pedestrian conflict.
- Utilize alleys for vehicular access where possible.
- Where off street parking is provided at rear or side, provide additional entry.





## APPENDIX B: TRAIL FUNDING



Potential Bicycle Facility Funding Sources in Wisconsin

Program	Purpose	Funding Details	App. Date	Notes	Administering Agency	Contact
Wisconsin Stewardship Programs						
Aids for the Acquisition and Development of Local Parks (ADLP)	To acquire or develop public outdoor recreation areas for “nature based” activities. Program rules being developed in 2000.	Program reauthorized and funding expanded for year 2000 as part of State Stewardship program.	May 1	A comprehensive outdoor recreation plan is required; priority is for land acquisition.	Wisconsin DNR	Duane Hofstetter (608) 266-5791 Eugene Park (414) 263-8676
Urban Greenspace Program (UGS)	To acquire land to provide natural space within or near urban areas, or to protect scenic or ecological features.	Program reauthorized and funding expanded for year 2000 as part of State Stewardship program.	May 1	A comprehensive outdoor recreation plan is required.	Wisconsin DNR	Duane Hofstetter (608) 266-5791 Eugene Park (414) 263-8677
Trails Program	To acquire lands for trails.	50% local match per project.	May 1	Funds available to organized conservation organizations.	Wisconsin DNR	Dave Hammer (608) 264-6034
Urban Rivers Grant Program (URGP)	To acquire lands or rights in land adjacent to urban rivers to preserve or restoring them for economic revitalization or outdoor recreation.	Program reauthorized and funding expanded for year 2000 as part of State Stewardship program.	May 1	A comprehensive outdoor recreation plan is required to participate.	Wisconsin DNR	Duane Hofstetter (608) 266-5791 Janet Beach Hanson (608) 266-0868
Land and Water Conservation Fund (LAWCON)						
	To acquire and develop public outdoor recreation areas and facilities.	50% local match per project. Wisconsin’s annual share of approximately \$740,000	May 1	A comprehensive outdoor recreation plan is required to participate.	Wisconsin DNR, with Federal funds.	Duane Hofstetter (608) 266-5791
National Recreational Trails Fund (RTA)						
a.k.a. “Symm’s Fund”	To provide funds for acquisition, maintenance, rehabilitation and development of both motorized and non-motorized and diversified trails.	Part of TEA-21. 50% local match per project.	May 1	Funding may only be used on trails which have been identified in or which further a specific goal of a local, county, or state trail plan.  Funds may be used on trails which are referenced in a statewide comprehensive outdoor recreation plan.	Wisconsin DNR, with Federal funds	Larry Friedig (608) 266-5797
Statewide Multi-Modal Improvement Program (SMIP)						
Bicycle and Pedestrian Facilities Program (BFPF)	For planning proposals and for facility development projects that implement a bicycle and/or pedestrian plan.	Part of TEA-21. 20% local match per project.	Feb 25	Large projects are encouraged.	WisDOT Bicycle/ Pedestrian Program Tom Huber (608) 267-7757	District 1: Dave McCosh, (608) 246-5445
Statewide Transportation Enhancements Program (STEP)	Enhance a state transportation project, including pedestrian and bicycle facilities, acquiring scenic easements, and preserving abandoned railway corridors. Local projects must be adjunct to a state highway.	Part of TEA-21. State will pick up half of the mandatory 20% local match.	Feb 25	Minimum project size is \$12,500; larger projects are encouraged. Program applications mailed November/December.	WisDOT Enhancements Program.	District 1: Dave McCosh, (608) 246-5445



Program	Purpose	Funding Details	App. Date	Notes	Administering Agency	Contact
Surface Transportation Discretionary Program (STP-D)	For projects that foster alternatives to single-occupancy vehicle trips.	Part of TEA-21. 20% local match per project. Program directed mainly at local communities.	Feb 25	Small projects, costing \$5,000 or more, are encouraged.	WisDOT STP-D Program	Mary Forlenza (608) 264-8724
Federal Transit Administration Grants						
Includes Section 3 discretionary funds.	Transit capital projects; includes intermodal facilities such as bike racks on buses and bicycle parking at transit stations; most funds are to be directed toward transit itself.	20% local match per project.	Early spring	Finding for this program is allocated on a discretionary basis. Congress/Administration can pick the projects although the authorization bill contains a list of specific earmarks.	WisDOT Bureau of Transit	Linda Lovejoy (608) 266-1379
Congestion Mitigation / Air Quality (CMAQ) Improvement Program						
	Funds projects which will reduce vehicle trips and miles; reduce emissions due to traffic congestion; or reduce the per mile rate of vehicle emissions.	50% local match per project.	Early April	Limited to Milwaukee, Kenosha, Racine, Ozaukee, Waukesha, Washington, Sheboygan, Kewaunee, Manitowoc, Walworth and Door Counties.	US DOT	John Duffy (608) 264-8723
Section 402 – Highway Safety Funds						
Community Programs	Funds bicycle and pedestrian education and enforcement projects and projects such as helmet purchase, sponsorship of bicycle rodeos, development of brochures, etc.	20% local match per project.	Dec 1	Contact WisDOT regarding the criteria for project selection. Engineering and maintenance work not eligible for funding.	WisDOT Office of Transportation Safety	Joanne Pruitt-Thunder (608) 267-3154
Highway Safety Program	Available for Bicycle/Pedestrian education. May also be used to develop safety classes for Bicycle/Pedestrian offenders.	20%-50% local match per project.	Dec. 1	Communities that can document bicycle crashes related to motor vehicle violations. Funds new enforcement programs up to \$1,000.	WisDOT Office of Transportation Safety	Joanne Pruitt-Thunder (608) 267-3154
Research Projects	Funds the research of data needed to substantiate unique local needs for safety funds.	Up to \$5,000 per project, 4 to 8 projects annually funded statewide.	1 <sup>st</sup> draft, Dec. 1	Participating communities combine enforcement efforts with an education program	WisDOT Office of Transportation Safety	Joanne Pruitt-Thunder (608) 267-3154
Wisconsin Main Street Community Program						
	Comprehensive downtown revitalization program which includes fundraising, business retention and marketing, volunteer development, public streetscape improvements, etc.	Technical assistance is available for 5 years. Inclusion in the program is competitive, with approximately 3 communities accepted into the program each year.	Late spring	Communities must be able to dedicate at least \$30,000 annually to the program.	National Main Street Center (202) 673-4219	Wisconsin Dept. of Commerce, Bureau of Downtown Development (608) 266-7531
Urban State Forestry Grant Program						
	To help communities increase their capability to manage threes through training, public awareness, inventory, tree planting, care and maintenance programs.	\$450,000 to \$500,000 available annually; \$1,000 to \$25,000 grants awarded with a 50% local match. Match may include in-kind services and donations.	Nov 1	50 to 60 grants made each year. Funds can be used for street tree planting if the communities demonstrate that this is its greatest need.	Wisconsin DNR Urban Forestry	Dick Rideout (608) 267-0843

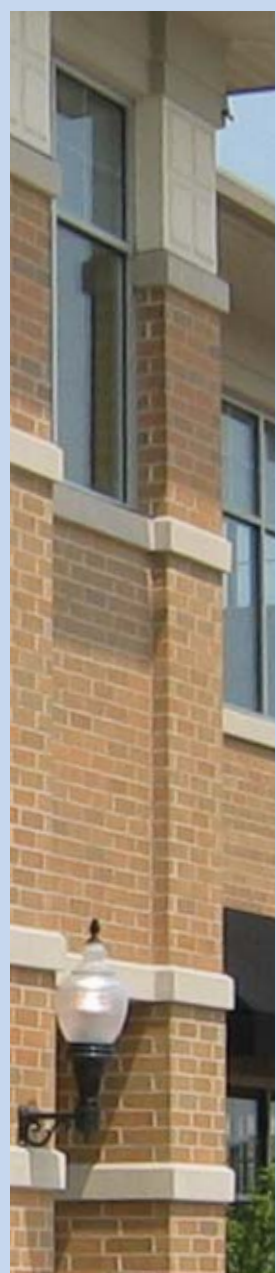
Source: *Jefferson County Bikeway/Pedestrianway Plan*, May 1996, Prepared by Camiros, Ltd., Madison, Wisconsin; Bicycle & Inc., Bolingbrook, Illinois; and R.A. Smith & Associates, Inc., Brookfield, Wisconsin; Updated by Vandewalle & Associates in August 1999, May 2000, and October 2003.





# VILLAGE OF SUSSEX Main Street Vision and Action Strategy

Adopted: July 26, 2011



Implementation Supplement to the 2004 Sussex Downtown Design and Development Plan







VILLAGE BOARD RESOLUTION #11- 26  
ADOPTION OF THE MAIN STREET VISION AND ACTION STRATEGY PLAN

WHEREAS, the Village of Sussex Downtown area also referred to as Main Street has sections where redevelopment is likely to occur particularly the areas of Main Street between Silver Spring and Maple Avenue and the intersection of Main Street and Waukesha Avenue.

WHEREAS, the Village of Sussex Community Development Authority has worked with Vandewalle & Associates to create a document to guide long range planning for the areas and named the plan Main Street Vision and Action Strategy Plan; and

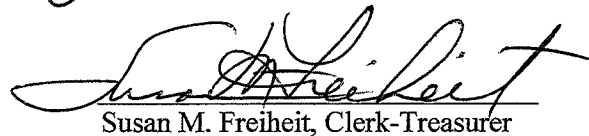
WHEREAS, the Village of Sussex Community Development Authority held a public open house to receive input from the business community and the public to help create the plan; and

WHEREAS, the Village of Sussex has determined that the adoption of the Main Street Vision and Action Strategy will ensure that the goals and objectives are consistent with current needs and trends.

NOW, THEREFORE, BE IT RESOLVED that the Village of Sussex Plan Commission on June 16, 2011 recommended the Village Board adopt a resolution to approve the Main Street Vision and Action Strategy Plan to be used as a working guide for the Community Development Authority.

Adopted this 26<sup>th</sup> day of July, 2011.

  
Greg L. Goetz, Chairperson

  
Susan M. Freiheit, Clerk-Treasurer

Number of ayes 6

Number of nays 0





# Acknowledgements

## SUSSEX COMMUNITY DEVELOPMENT AUTHORITY

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## Relationship of this document to the Downtown Design and Development Plan

This document was created by the Village Community Development Authority (CDA) and adopted by the Village Board as an implementation supplement to the Downtown Design and Development Plan. However, it is not a formal part of that Plan or the Village Comprehensive Plan. Instead, this is a policy document intended to guide the actions of the CDA as it seeks to implement those Plans through its own actions and those undertaken jointly with property owners and developers.

Given the length of time that has passed since the last formal update of the Downtown Plan and the significant changes that have occurred in the real estate and development markets, the CDA thought it was important to revisit the vision for two specific areas of the downtown now referred to as Sussex Center and Cannery Crossing. In addition, specific development objectives and example concept designs also were developed for these areas in order to gain a greater understanding of the implementation requirements. However, the contents of this document are intended to be generally consistent with the contents of the adopted Downtown Plan, and property owners and developers should view this document as simply providing more detail for these two areas. Any actual or apparent conflicts between this document and the Downtown Plan should be discussed with Village staff and the CDA to gain clarity on the preferred direction.



*Open House Attendees review planning materials on February 3, 2011*

A final outcome of this effort is the identification of 15 different projects, which the CDA has prioritized in terms where it intends to direct its efforts on a proactive basis. The CDA welcomes additional projects as proposed by property owners and developers, but in the absence of those the CDA will focus on the priorities identified in this document. Further, the CDA will revisit all of the projects twice a year and amend the Implementation Action Plan at least once per year based on the then prevailing development environment and the prioritization criteria included in this document.



*Over 40 attendees met with Village Staff, CDA Members, and the planning consultant*



## Introduction

The Village's development of a Vision and Action Strategy for the central Main Street area is a long-range planning effort intended to update and augment the Downtown Design and Development Plan and to help guide the work of the Village's Community Development Authority (CDA). The planning process provided an opportunity for the community to envision how the Main Street area would best look, feel, and function in the year 2030. The process included collaborative planning and design work between Village Staff and their development consultant for this project, Vandewalle & Associates; meetings with the CDA to discuss the areas and review draft documents; and a Public Open House in which over 40 members of the public came together to discuss the project areas and review draft materials.

This document focuses on two distinct areas of Main Street: the stretch between Silver Spring Drive and Maple Avenue, referred to here as "Sussex Center", and the area around the intersection of Main Street and Waukesha Avenue, referred to here as "Cannery Crossing". The two areas are shown on the Planning Areas Map on Page 3. Note that the names for these areas are only suggestions. In particular, it is expected that the ultimate developer of the "Cannery site" will have significant input into the name of that site and the area around it.

**Sussex Center** includes the area around the Village Hall, Library, and Weyer Park and extends generally along Main Street between Maple Avenue on the west and Youth Hall/Silver Spring Drive on the east. As indicated in the Vision Statement for this area (Page 5), Sussex Center is envisioned in 2030 to serve as the Village's "living room"—a center of civic life and community activities. The area includes a mix of public facilities and businesses that meet many of resident's daily needs. The area is attractively maintained and its buildings reflect a mix of old and new development.

**Cannery Crossing** includes the former Cannery site, the Quarry, and the properties across the street along both Main Street and Waukesha Avenue. As indicated in the Vision Statement for this area (Page 9), Cannery Crossing is envisioned in 2030 as a dynamic crossroads development area that acts as an eastern gateway to Main Street. Notable mixed-use development leverages the former cannery's prime location and provides residents and passers-by with a mix of retail, offices, dining, and multi-family housing. The area is also an important recreational asset as the restored Sussex Creek and Quarry Pond—connected by the Bugline Trail to the larger area—provide valuable natural spaces for the community.

While these two areas are near one another and are connected economically and physically, they have different current and anticipated future land uses and fill different roles within the community and region. Recognizing this, the CDA decided to create a distinct plan—including a vision, development objectives, conceptual development plans, and implementation strategy—for each area. These products, compiled in this document, are intended to help guide the CDA in working with property and business owners considering redevelopment options, and to guide the Village in considering

future public improvements in these areas. The Vision Statements present a description of what this area may be like in the year 2030. The Village will use these Statements to guide visionary, yet realistic and attainable improvements that build off the area's existing assets. The Development Objectives are guidelines that all projects in these areas should follow. They align with the Village's goals and vision for their respective areas. The Conceptual Development Plans show examples of how new development or improvements may be designed to meet the Objectives. Two alternatives are provided for each area, but the sub-area components from each can be mixed and matched (and new ones that meet the Development Objectives introduced) as may be desired by property owners/developers and the CDA. The Main Street Vision graphics show how the Conceptual Development Plans fit together within the full context of Main Street from Maple to Waukesha Ave. Lastly, an Implementation Strategy has been included. An implementation prioritization process was created to help guide the CDA's work, and priorities will be reconsidered on a biannual basis to ensure the CDA's efforts adapt to the changing development market and new opportunities.

### SUMMARY OF OPEN HOUSE COMMENTS

Over 40 members of the public, including Main Street business and property owners, attended the Open House held February 3, 2011. The Open House was an informal event where attendees could review planning materials around the room, complete comment sheets, and discuss the work with members of the CDA, Village Staff, and the project consultant. A summary of comment sheet responses is provided on Page 17.

In reviewing completed comment forms and conversations held, the Village found there was a high level of agreement with the vision, development objectives and concept plans for both planning areas. For Sussex Center, all elements of the Vision received a high degree of support. Top priority projects of the community included the Grocery Store Site and the Main and Silver Spring Intersection. Most attendees supported components from both Conceptual Designs A and B rather than supporting either A or B in its entirety. For Cannery Crossing, there was a majority of support for taller buildings and apartments although there were participants who were adamantly opposed to both. Top priority projects identified for Cannery Crossing included Quarry Pond restoration, redevelopment of the Cannery Site, and improvements/realignment of the Bugline Trail. Attendees had a clear preference for Conceptual Design A, which was similar to B in many respects but showed the Creek realigned to its historic location. Many attendees also expressed concern that newer residents and visitors may not understand the reference to the cannery, and recommended "Mammoth Springs" be referenced in the name instead.

Overall, the Open House found very little disagreement regarding the written development objectives for each area. While the Village can expect differences in public opinion regarding specific proposals, it appears that the written objectives reflect well the common priorities of the public for these areas. They will serve as a useful guiding tool for discussions with business owners and developers regarding project reviews and development negotiations.



# PLANNING AREAS SUSSEX, WI



Created: January 19, 2011  
Vandewalle & Associates, Inc.  
© 2010











**THE VISION:** Sussex Center is the Village’s “living room” hosting a variety of civic activities, community events, outdoor recreation opportunities, and cultural programming. The Center’s many public facilities are complemented by a mix of businesses catering to residents’ daily needs. High quality buildings, a mix of historic and modern, convey the community’s rich history and forward thinking outlook. The area is inviting and appealing to residents and visitors alike.



**WHAT WILL BE HERE?** Located in the heart of the Village, Sussex Center is a vibrant area of public life. A rich set of public facilities including the Library, Village Hall, Weyer Park, Old Brooke Square, Sussex Creek, and Youth Hall are highly utilized. Extending west from this civic core to the “Four Corners” of Main and Maple is a diverse mix of complementary retail and commercial businesses. Businesses range from a grocery and medical offices to specialty retailers and restaurants.

Infill development is thoughtfully integrated into the existing community fabric, leveraging and adding new value to the core functions of Sussex Center. Continued reinvestment and the overall cohesive, attractive, and welcoming appearance throughout the Center demonstrate community pride.

Together, these public and private developments create an identifiable district that meets the daily needs of residents and serves as a center point for interaction between neighbors. Residents enjoy the convenience of having many daily needs, as well as specialty destinations, available within the community. The choice to meet many needs without a car has increased the attractiveness of new residential development in the area and in adjacent older neighborhoods.

**WHAT WILL IT LOOK LIKE?** Sussex’s Center’s character and visible heritage are central to its success. Through its Design Guidelines as well as careful code enforcement, the Village works to mix old and new, promote cohesive growth, encourage high-quality, lasting construction, and prevent blight. The result is a visually-distinct and activity-rich area offering a strong contrast to typical suburban development.

Visual cohesiveness in Sussex Center is achieved through thoughtful and integrated site layout standards; use of materials that set a standard of quality and artisan feel, such as Lannon Stone and wrought iron; and through quality and consistent streetscaping including landscaping, banners, seasonal decorations, appropriately-scaled lighting, and well-marked intersections. Likewise, public facilities are of the highest standards. Through diligent maintenance and sensitive additions to historic structures like Youth Hall, and through quality newer buildings like the library, Sussex Center’s public buildings reflect the pride in and importance of these amenities to the community. This sentiment is also expressed through the parks and open spaces within Sussex Center, including the restoration of Sussex Creek and Weyer Park improvements such as an amphitheater, picnic areas, walking trails, and seasonal landscaping.



**HOW WILL IT FIT INTO THE COMMUNITY?** Sussex Center helps promote the community’s small-town character. Aided by the Center’s appeal, Sussex is regionally-known as a highly-desirable, active community in which to live, do business, raise a family, or retire. Whether traveling Main Street by car or on foot, it is evident Sussex is a well-established, prosperous, and inviting community.

With good connections to new residential development, existing neighborhoods, and Cannery Crossing via Main Street and the Bugline Trail, Sussex Center can be reached by walking or biking as well as by driving. While Main St. is heavily-traveled by Village residents, area employees, and those passing through town on their way to the lakes, clearly-delineated paths and well-maintained infrastructure facilitate safe access and help Sussex leverage its traffic flow to build its name and image and attract businesses and customers.





## Sussex Center Development Objectives

### CIVIC CAMPUS

- Restore creek channel, improve vegetation, and enhance stormwater management reducing the 100 year floodplain
- Add trails on both sides of the creek through Weyer Park to Old Mill Lane
- Add other amenities like seating, lighting, picnic areas, etc. to increase daily use
- Add amphitheater/restrooms to host events
- Consider relocation of the train depot & museum to the Campus to attract more visitors
- Consider reconfiguration of parking areas to increase efficiency, add spaces, and provide more useable green space
- Work with neighboring bank to improve the creek all the way to Main St. and add attractive landscaping east of Village Hall
- Look for other ways to “bring the Park to the street” through increased landscaping, perhaps as part of a Village Hall relocation/reconstruction

### GROCERY STORE SITE

- Work with owner to evaluate expansion/redevelopment options
- Owner to evaluate additional land acquisition
- Improve building exterior consistent with adopted design guidelines
- Screen parking lot along Main Street consistent with adopted design guidelines
- If site is expanded, look to add retail building on east side
- Provide stormwater treatment along east side consistent with creek restoration

### MAIN AND SILVER SPRING INTERSECTION

- Evaluate potential to realign to a 90° intersection
- If intersection is realigned, evaluate relocation of the creek to south side of Silver Spring Rd.
- If intersection is realigned, add cul de sac to end of Pembroke St. with trail connection to Bugline
- If intersection is realigned, aggregate property in new SE quadrant for substantial redevelopment project and expanded parking for Youth Hall
- If intersection is realigned, maintain general location of Bugline trail and integrate into new development
- If intersection is realigned, provide a community/Sussex Center gateway feature at the new corner
- If intersection is not realigned, evaluate potential to expand Sussex Square to the east and add parking to serve Youth Hall

- If intersection is not realigned, evaluate potential to relocate train depot & museum to an expended Sussex Square

### YOUTH HALL

- Evaluate potential to acquire property to the west to provide expansion space and parking area
- If additional land is acquired, separate parking area from drop-off area
- Evaluate potential to acquire property across Silver Spring Rd. for additional parking

### BUGLINE TRAIL

- Use trail to connect redevelopment sites in area
- Use trail to connect area to Cannery Crossing and other parts of community
- Providing wayfinding and information signage to direct visitors

### NORTH SIDE OF MAIN, EAST OF ORCHARD

- Encourage property assembly to provide more parking for vacant commercial building
- Encourage property assembly to accommodate more significant redevelopment
- Mixed-use development serving local and regional needs
- Ground floor commercial with office or residential above
- Orient buildings to Main St.
- 2-3 stories
- Building designs and materials consistent with adopted design guidelines
- Incorporate “green building”/sustainable features
- Minimize access points along Main Street (1 per property, shared where possible)
- Parking only in the rear/along the side
- Access from Hillview Drive to serve only residential uses

### MAIN STREET WEST OF CIVIC CAMPUS/GROCERY STORE

- Encourage property assembly and/or commercial/office redevelopment over time
- Orient buildings to Main St.
- 2-3 stories
- Building designs and materials consistent with adopted design guidelines
- Incorporate “green building”/sustainable features
- Seek to consolidate/minimize vehicular access points (1 per property, shared where possible)
- Locate future development outside of floodway



# SUSSEX CENTER

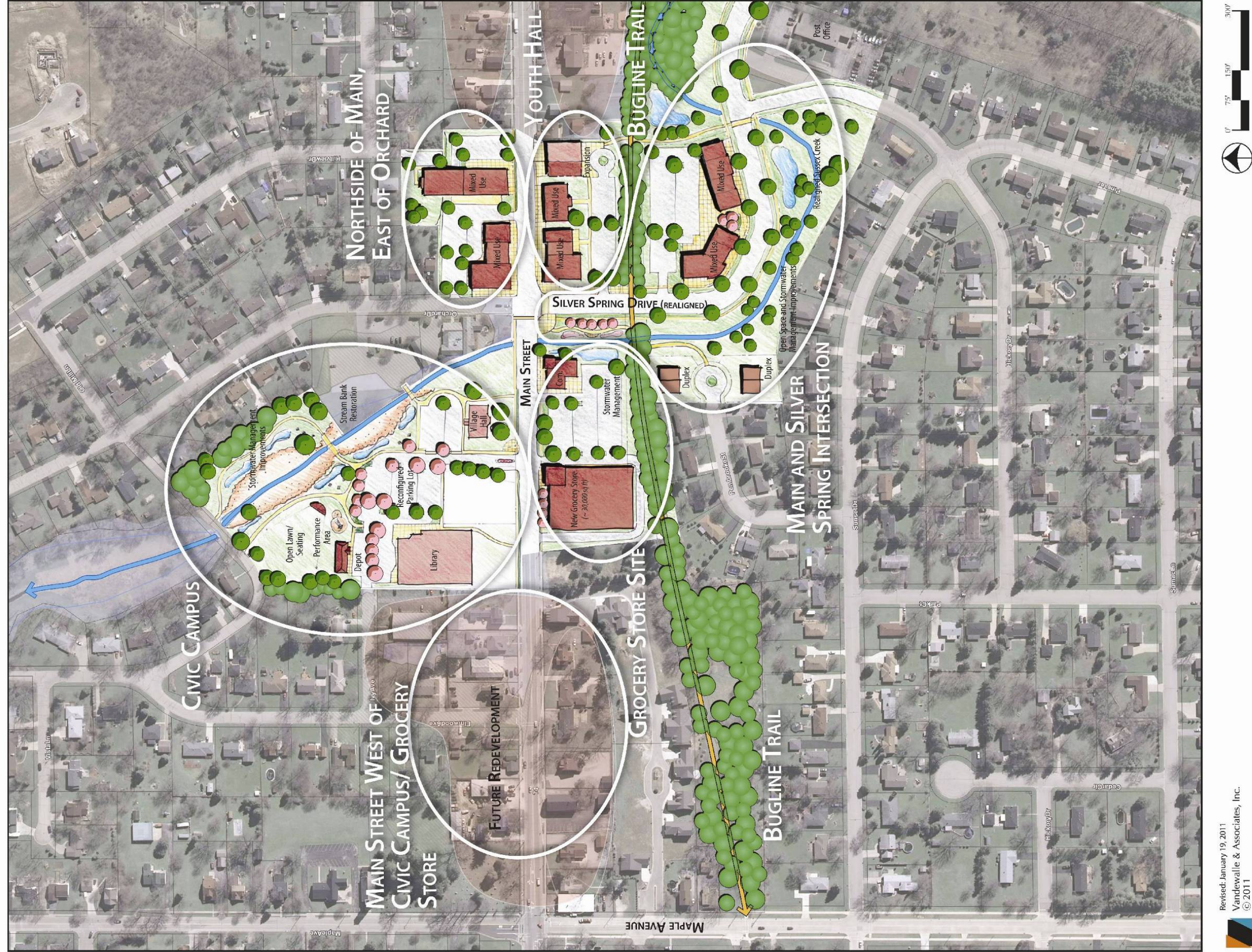
# CONCEPT A



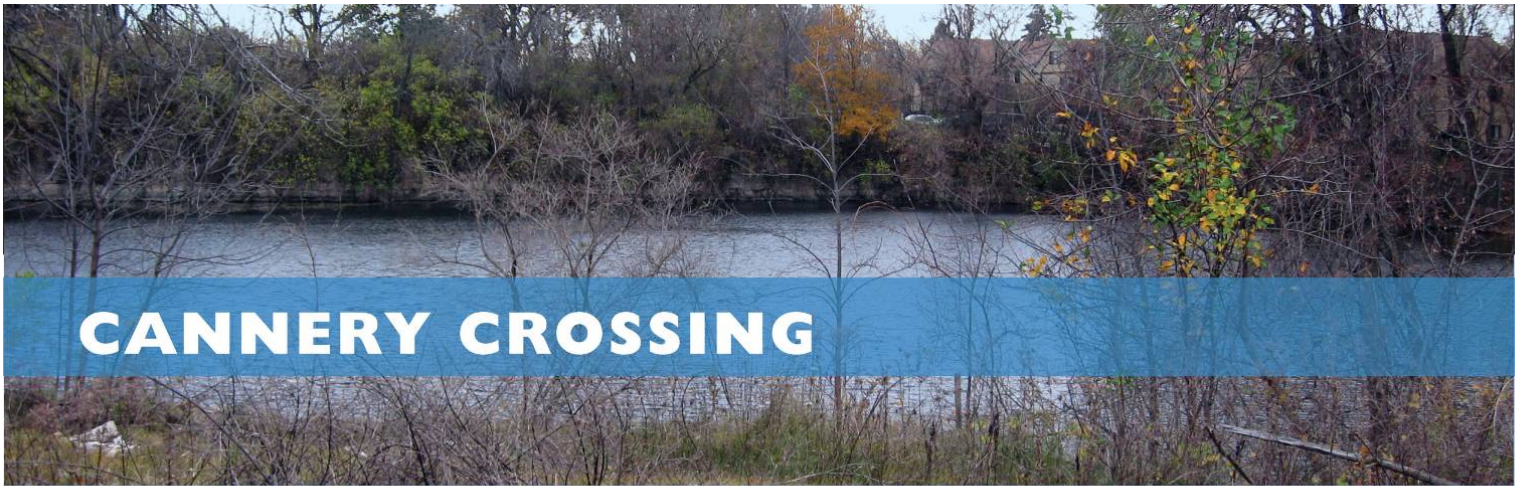


# SUSSEX CENTER

## CONCEPT B







# CANNERY CROSSING

# Imagine 2030...



**THE VISION:** Cannery Crossing is a dynamic crossroads and Main Street’s eastern gateway. Anchored by an eye-catching, new mixed-use development on the former cannery site, Cannery Crossing offers a mix of retail, offices, dining, recreation, and multi-family housing for Village residents, area workers, and visitors. The restored Sussex Creek, Quarry Pond, Bugline Trail, and new “green” construction demonstrate Sussex’s commitment to both vibrant urban placemaking and sustainability.



**WHAT WILL IT LOOK LIKE?** Eye-catching, multi-story development is oriented to the intersection and uses sustainable, quality construction materials. An inviting and dynamic plaza captures the interest of passers-by and serves as a gatepost to Sussex for those approaching from the east. Streetscape emphasizes the area as a community gateway and visually connects Cannery Crossing to Sussex Center via Main Street. A comprehensive and integrated wayfinding system also directs visitors to various points of interest within the Village. Environmental conditions have improved, including removing development from the Sussex Creek floodway and flood-plain; daylighting and restoring Sussex Creek north and south of Main Street; shoreline improvements and pier construction at the Quarry Pond; rerouting the Bugline Trail to the south shore of the Pond; improving amenities at the Bugline Trailhead; and incorporating state-of-the-art stormwater management into all new development. The result is an accessible, vibrant and attractive urban environment that is interwoven with and complimentary to the area’s natural features.

**WHAT WILL BE HERE?** Cannery Crossing includes a compact mixture of new commercial and multi-family residential development of high architectural quality that utilizes green building technologies.

At the former Cannery site, a major redevelopment supports 4-6 story buildings. Oriented to the corner of Main St. and Waukesha Ave., ground-floor commercial development is topped with offices and residences. In the site’s interior, development ranges from 2-6 stories, with commercial uses concentrated toward the northeast corner and residential or office uses adjacent to the Quarry Pond. The Cannery site’s size has resulted in a phased development with a variety of new uses, including new housing to meet emerging demands such as for high-quality apartments and condominiums for young professionals and empty-nesters.

The Village has worked with the site developer to reclaim the Quarry for public recreation—developing a walking trail, scenic overlook, fishing pier, and green space. The Quarry’s safety and appearance have been improved, drawing visitors while minimizing liabilities. In addition, the Bugline Trail has been relocated around the Pond to improve its accessibility.

Across Main St., underutilized properties have been assembled and redeveloped to serve as a complementary gatepost to the Cannery site. New development includes two- to four-story green commercial buildings that take advantage of their location adjacent to the restored Sussex Creek and an enhanced Bugline Trailhead, which includes public parking.

Interest in additional new development along both sides of Waukesha Ave., between Silver Spring and Main St., is growing. New development here consists of compact, interconnected retail, restaurants, and offices that cater to both passing traffic as well as Village residents and nearby employers.

**HOW WILL IT FIT INTO THE COMMUNITY?** At its prominent location along heavily-traveled Waukesha Ave., Cannery Crossing contributes economically and visually to the community in its own right, while also anchoring the eastern end of Main St. Development of this key crossroads has established a sense of place along Waukesha Ave. and attracts motorists’ attention both to the development and down Main St. toward Sussex Center.

While Cannery Crossing and Sussex Center are distinct in their development type and function, they are visually connected through the Village’s streetscape improvements along Main St. Market-driven infill redevelopment between the two areas is also picking up pace. By increasing the population base in the heart of the community and generating new activity on Main St., Cannery Crossing also supports Sussex Center’s role as the vital center of daily activity and civic life.





## Cannery Crossing Development Objectives

### CANNERY SITE

- Mixed-use with retail/restaurant on ground floor with office and/or residential above
- Orient buildings in front to Main and Waukesha with minimal setbacks
- Residential or office uses adjacent to the quarry pond
- 2 – 6 story buildings
- Building design and materials consistent with adopted design guidelines
- Integrate significant “green building”/sustainable features
- Minimize vehicular access points (no more than 2 on Main and 1 on Waukesha)
- Provide parking in/under buildings
- Surface parking only in rear/along sides and broken into smaller lots
- Attractive and inviting public plaza space at corner
- Community gateway feature at corner
- Restore creek and provide trail consistent with improvements on north side of Main
- Maintain Bugline trail through the site (may be relocated along quarry)
- Maintain utility easement through the site (along current trail alignment)
- Provide public access to the quarry and integrate site development with recommended quarry improvements

### QUARRY RESTORATION

- Obtain public trail access easements around the entire quarry
- Quarry pond safety – combination of attractive fence, berm, and landscaping
- Provide a pier or boardwalk to permit fishing at west end
- Provide an overlook on south side and potential public parking area

### BUGLINE TRAIL

- Use trail to connect redevelopment sites in area and provide access to quarry
- Use trail to connect area to Sussex Center and other parts of community
- Add trail connection to the south to Spring Green Park
- Provide wayfinding and information signage to direct visitors

### DEYER DRIVE REDEVELOPMENT

- Medium density residential development
- Building types compatible with single family homes on street, such as townhomes, attached villas

- Maintain front setbacks similar to single family homes
- Consider senior housing
- Maximum of 2 ½ stories
- Building designs and materials consistent with adopted design guidelines
- Incorporate “green building”/sustainable features
- Parking in garages, or surface lots only in rear/along side and in small lots
- Garages not to extend beyond front walls of living units
- Integrate with recommended quarry improvements

### NORTH SIDE OF MAIN STREET

- Daylight creek to reduce flooding, restore habitat, and attract new development
- Encourage land assembly and redevelopment consistent with new creek alignment and revised limits of floodplain and floodway
- Orient buildings to Main Street
- Commercial development serving local and regional needs
- 2-4 stories
- Building designs and materials consistent with adopted design guidelines
- Incorporate “green building”/sustainable features
- Minimize access points along Main Street (1 per property, shared where possible)
- Parking in/under buildings to increase development potential
- Surface parking only in the rear/along side in small lots
- Integrate restored creek as an amenity (patio area, etc.)
- Consider additional amenities at county trail parking area (seating, shade/rain shelter, etc.), or create regional trailhead at Madeline Park

### WAUKESHA AVENUE BETWEEN MAIN AND SILVER SPRING

- Encourage land assembly and/or commercial/office redevelopment over time
- Orient buildings to the street with minimal parking in the front
- 2-4 stories
- Building designs and materials consistent with adopted design guidelines
- Incorporate “green building”/sustainable features
- Minimize vehicular access points (1 per property, shared where possible)
- Parking in/under buildings to increase development potential
- Break surface parking into smaller lots
- Integrate with recommended quarry improvements where adjacent



# CANNERY CROSSING

# CONCEPT A





# CANNERY CROSSING

## CONCEPT B





# CONCEPT A

# MAIN STREET VISION SUSSEX, WI



Revised: February 3, 2011  
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# Village of Sussex CDA Main Street Area Implementation Priorities

## PRIORITIZATION CRITERIA

Priority projects are those that:

1. Have already started, are making progress, and have established momentum for moving ahead
2. Are more cost effective and efficient to advance simultaneously with another project(s) already identified as a priority
3. Continue/build off of a previous project
4. Have funding source(s) available that are specific to the project/are economically viable
5. Have potential to serve as a catalyst that will accelerate other identified projects
6. Result in the development of more detailed plans, establishment of policies, or decisions necessary for other implementation activities to be undertaken
7. Have identified partners who are willing and able to participate
8. Have the strong support and interest of the CDA, Village Board, and citizens

## PRIORITIZATION LEVELS

Projects are broken into the following priority levels based on their extent of consistency with the Prioritization Criteria.

- Level 1 –** These are projects that meet five or more of the criteria and should be the primary focus of the proactive efforts undertaken by the CDA and Village.
- Level 2 –** These are projects that meet three or four of the criteria. The CDA should continue to monitor conditions and trends related to these (such as the completion of related projects, availability of funding, market conditions, etc.) to determine when they comply with the Level 1 criteria and are more ripe for attention and action.
- Level 3 –** These are projects that meet less than three of the criteria. Current conditions and trends will need to change significantly before these would increase in their level of priority. Many of these will be market driven and are dependent upon a property owner/business/developer initiating them. Once that happens, the project is likely to jump to a Level 1 Priority. Thus, classifying a project as a Level 3 Priority does not indicate the CDA is not interested in it; it means the CDA believes current conditions and trends require another party to take the lead.

## MAIN STREET IMPLEMENTATION PROJECTS

Based on the above, all of the projects identified in the Main Street planning process have been classified into three prioritization levels. The numbers within the parentheses that follow each item are the Priority Criteria that apply to each item.

### Level 1

- ♦ Sussex Creek Restoration (1, 2, 4, 5, 6, 7, 8)
- ♦ Grocery Store Site (1, 4, 5, 7, 8)
- ♦ Cannery Site, Quarry Restoration & Bugline Trail Improvements (2, 5, 6, 7, 8)
- ♦ Main and Silver Spring Intersection (2, 4, 5, 6, 8)
- ♦ TIF (2, 3, 4, 5, 6, 8)

### Level 2

- ♦ Civic Campus (2, 3, 7, 8)
- ♦ Youth Hall (3, 6, 7)
- ♦ Village Wayfinding Program (2, 4, 7)
- ♦ Bugline Trailhead Improvements (1, 2, 3, 7)

### Level 3

- ♦ East Side of Waukesha
- ♦ Main Street West of Civic Campus/Grocery Store
- ♦ North Side of Main, East of Orchard
- ♦ Deyer Drive Redevelopment

## IMPLEMENTATION MANAGEMENT

The CDA should informally review and evaluate all projects every six months and formally amend its work plan once a year.



Sussex Center and Cannery Crossing  
Implementation Action Plan 2011 - 2013  
March 2011

	Heavier Level of Effort
	Lighter Level of Effort/Ongoing

ACTION ITEM	Lead Organization	Partner Organizations	2011										2012											
			Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	
LEVEL 1 PRIORITIES																								
Sussex Center																								
1.A. Grocery Store Site																								
Continue working with owners to determine needs and options	Store Owner	CDA																						
Assist owner in determining potential for property acquisition to the west	Store Owner	CDA																						
Investigate relocation of depot to civic campus	CDA	Historic Society																						
Work with owners on exterior design	Store Owner	CDA																						
Work with owner on site improvements (landscaping, screening, parking, walkways)	Store Owner	CDA																						
Work with owners on stormwater options	Store Owner	CDA																						
Assist owner in determining potential for additional development on east side of property	Store Owner	CDA																						
Investigate designs and costs for restoration of the creek	CDA	Parks																						
Negotiate development agreement with owners	Store Owner	CDA																						
Approve development agreement	Village Board	CDA																						
Approve needed zoning	Village Board	Plan Commission, CDA																						
1.B. Main and Silver Spring Intersection																								
Work with Village Engineer to evaluate feasibility and costs of realignment	CDA	Village Engineer																						
Prepare construction schedule and funding strategy	CDA	Village Engineer																						
Decide whether to move ahead with construction	Village Board	CDA																						
Cannery Crossing																								
1.A. Sussex Creek Restoration																								
Continue working with DNR on preferred concept	DNR	CDA, Parks																						
Work with DNR on cost estimates	DNR	CDA, Parks																						
Work with DNR on funding strategy	DNR	CDA, Parks																						
Determine interest of affected property owners	DNR	CDA, Parks																						
Decide whether to acquire property and move ahead	DNR, Village Board	CDA, Parks																						
1.B. Cannery Site																								
Work with prospective developer on development concept	Developer	CDA																						
Uses																								
Design																								
Sustainability components																								
Creek restoration																								
Bugline Trail relocation																								
Access points																								
Plaza design and amenities																								
Integration with Quarry																								
Phasing																								
Negotiate development agreement	Developer	CDA																						
Approve development agreement	Village Board	CDA																						
Approve needed zoning	Village Board	Plan Commission, CDA																						
1.C. Quarry Restoration																								
Determine access needs around all sides to accommodate a continuous trail	Parks	CDA, Cannery Developer																						
Meet with adjoining property and determine impacts and costs to obtain easements	CDA	Parks, Cannery Developer																						
Decide whether to obtain easements	Village Board	Parks, CDA																						
Determine perimeter security needs and potential designs	Parks	CDA, Cannery Developer																						
Determine needs/designs for pier, overlook and other improvements	Parks	CDA, Cannery Developer																						
Prepare cost estimates	Parks	CDA, Cannery Developer																						
Prepare funding strategy including participation by Cannery Developer	Parks	CDA, Cannery Developer																						
Include Quarry improvements in development agreement for Cannery as appropriate	CDA	Cannery Developer																						
1.D Bugline Trailhead Improvements																								
Based on creek realignment, work with county to determine improvements	Parks, CDA, County	DNR																						
Prepare funding strategy	Parks, CDA, County	DNR																						
Decide whether to construct improvements	Parks, CDA, County																							
General Implementation																								
1.A. TIF																								
Determine potential TIF boundaries	CDA	TIF Consultant																						
Determine TIF adoption schedule/triggers	CDA	TIF Consultant																						
Prepare TIF plan	CDA	TIF Consultant																						
Conduct TIF hearings and JRB meetings	CDA	TIF Consultant																						
Decide whether to adopt TIF	Village Board, JRB	CDA																						

# CDA Open House Public Comment Summary – Sussex Center

## FEBRUARY 3, 2011

### COMMENT SHEET RESULTS: "SUSSEX CENTER"

As you make your way through the displays, please consider the following questions:

**Sussex Center Vision** — The Vision presents a description of what this area may be like in the year 2030. The Village will use this to guide visionary, yet realistic and attainable improvements that build off the area's existing assets. As you review, rate your agreement with each section:

	4	3	2	1	
	Support strongly	Support most elements	Disagree with some elements	Do not support	
	4	3	2	1	Average
The Vision statement	6	9	1	2	3.06
What will be here?	5	8	3	2	2.89
What will it look like?	5	9	2	2	2.94
How will it fit into the community?	5	9	1	2	3.00

**Additional Thoughts:**

- Geographical center of village.
- The Village Hall - a signature building of our community should remain and added to - the Save Our School Committee had the right idea in 1988-1990. The 1922 High School/Main Street School should remain for hundreds of years. This is the reason why there was/is the Sussex Fire Dept (1922) and because the fire department was founded because of this fire the same men incorporated the Village of Sussex in 1924. It is our icon, our logo.

**Development Objectives & Conceptual Development Plans** — Once adopted by the Village, the Development Objectives become guidelines that all projects should follow. Therefore, it's important they align with the Village's goals and vision for the area. In contrast, the two Conceptual Development Plans will not be followed precisely—they simply show concepts of how new development or improvements may be designed to meet the Objectives.

As you review, please rate your agreement with each section of the Development Objectives and indicate which Conceptual Development Plan (A or B) you prefer for each area:

Priority:		Agreement:					Prefer:	
#	Development Objectives	4	3	2	1	Average	A	B
4	Civic Campus	9	6	0	1	3.44	7	5
6	Grocery Store Site	5	8	0	0	3.38	3	8
6	Main & Silver Spring Intersection	6	6	1	1	3.21	5	6
3	Youth Hall	4	9	3	0	3.06	6	6
4	Bugline Trail	6	9	0	0	3.40	4	5
2	North Side of Main, East of Orchard	4	8	1	0	3.23	2	9
0	Main St west of Civic Campus/Grocery Store	6	8	1	0	3.33	4	5

Next, please put a check mark by the 1-3 areas above you think should be the Village's priorities for this area.

**Additional Thoughts:**

- Do something to allow expansion of grocery store. Moving depot looks good. Option of realigning SS is good. West of Campus good for condos, townhomes, professional businesses.
- The youth hall should be expanded, but within the framework of Plan A.
- Really don't like SS reroute that little salon is kind of an eye sore.
- My concern is rerouting Silver Spring. I believe it will cause more traffic on Sunset
- Pedestrian important
- I would like to combine both ideas. I don't want to limit the traffic flow. If anything, increase it to promote more growth. I would like to see the Pig expand and move the historic access street.
- To change Silver Spring - Main Street intersection would be very expensive and impede traffic flow to areas you want to draw to.

**Area Name:** The Village is referring to the area in question as "Sussex Center" for planning purposes as it seemed to be a good descriptor of the area and a name which could be used in the future for marketing/promotions. Clearly other names could also work well. Please indicate whether you think we should use Sussex Center, a different idea below, or another idea:

Sussex Center (2)	Sussex Square (6)	Sussex Crossing (0)
Civic Center (0)	Sussex on the Creek (1)	Main Street (0)
Village Center (3)	Sussex Downs (2)	Sussex Main (0)
Other (3) Sussex Town Square; Sussex Civic Center		

**Additional Comments:**

- What becomes of historic Zion Evangelical Church? Can't some use be found for it, expansion of museum? Annex?
- Save the Village Hall/Main Street School. Have referendum, I would abide by vote.
- If you call it Sussex Center or something that says the center of civic activities, it should have most or all of the Village services a resident would need. Keep as many things together so people don't need to travel to different parts of town to conduct Village business.

# CDA Open House Public Comment Summary – Cannery Crossing

## FEBRUARY 3, 2011

### COMMENT SHEET RESULTS: "CANNERY CROSSING"

As you make your way through the displays, please consider the following questions:

**Cannery Crossing Vision** — The Vision presents a description of what this area may be like in the year 2030. The Village will use this to guide visionary, yet realistic and attainable improvements that build off the area's existing assets. As you review, rate your agreement with each section:

	4	3	2	1	Average
	Support strongly	Support most elements	Disagree with some elements	Do not support	
The Vision statement	8	9	4	0	3.19
What will it look like?	6	10	4	1	3.00
What will be here?	6	9	6	0	3.00
How will it fit into the community?	8	7	5	0	3.15

#### Additional Thoughts:

- Concerned about traffic pattern at left turn lane into Cannery from northbound 74.
- Allow fishing and small boats for fishing in quarry - no swimming.
- No apartments!
- No multi-family units on Deyer Drive
- Use Sussex Lake as an anchor - develop around it.

**Development Objectives & Conceptual Development Plans** — Once adopted by the Village, the Development Objectives become guidelines that all projects should follow. Therefore, it's important they align with the Village's goals and vision for the area. In contrast, the two Conceptual Development Plans will not be followed precisely—they simply show concepts of how new development or improvements may be designed to meet the Objectives.

As you review, please rate your agreement with each section of the Development Objectives and indicate which Conceptual Development Plan (A or B) you prefer for each area:

Priority:		Agreement:					Prefer:	
#	Development Objectives	4	3	2	1	Average	A	B
9	Cannery Site	6	7	1	1	3.20	11	2
10	Quarry Restoration	10	6	0	1	3.47	12	2
8	Bugline Trail	7	8	1	1	3.24	12	2
1	Deyer Drive Redevelopment	2	9	2	3	2.63	9	3
4	North Side of Main Street	5	7	2	3	2.82	12	3
1	Waukesha Ave between Main & Silver Creek	4	8	2	2	2.88	10	2

Next, please put a check mark by the 1-3 areas above you think should be the Village's priorities for this area.

#### Additional Thoughts:

- Creak flow looks more natural
- Make the Quarry Pond the center piece of a retail "boardwalk"
- The back and forth of the Bugline (at Main St) is a huge pain when you are on a bike (or snowmobile). Think direct path.
- Two story maximum at canning site.
- Traffic coming in/out of crossing would impede already congested area - otherwise great.
- Would love to shop and eat in town and support local business, but don't feel it should include residential area within shopping area.

**Area Name:** The Village is referring to the area in question as "Cannery Crossing" for planning purposes as it seemed to be a good descriptor of the area and a name which could be used in the future for marketing/promotions. Clearly other names could also work well. Please indicate whether you think we should use Cannery Crossing, a different idea below, or another idea:

Cannery Corners (1)	Main & Waukesha (0)	Templeton Crossing (1)
East Village (0)	Quarry Crossing (1)	Templeton Corners
Mammoth Springs (10)	Quarry Corners (1)	
Other (2), Village East, Sussex Springs		

#### Additional Comments:

- Leave the cannery reference for the commercial development
- Please, no parking lot access on ONLY Kneiske. Option to travel around quarry while on Bugline. We still need a gas station on Main. Don't eliminate both! We've lived here for 10 years and a long time resident had to explain the "Cannery". If you go with that you have to do something to show why, otherwise as generations go on nobody will get it.
- Cannery is no longer on the property - only older residents know of the canning operation.